

**Environmental Analysis
Okauchee Lake Public Boat Access Renovations
Waukesha County, Wisconsin
August 25, 2014**

PROJECT SUMMARY

1. Overview

The Department of Natural Resources proposes to renovate the existing public boat landing facility on Okauchee Lake in the Town of Oconomowoc. Work at the site would include improvements to the parking lot which are intended to improve traffic flow through the site and to relieve traffic congestion on Road 'T'. Road 'T' is the dead-end town road which terminates at the existing boat launching facility. A cul-de-sac would be constructed at the end of Road 'T' to allow vehicles using Road 'T' to turn around without entering the boat landing area. The existing boat launch parking lot would be reconfigured and would include an additional six car/trailer parking stalls and an additional five car only parking stalls. A fishing pier is also proposed to be constructed at the site. An older non-accessible concrete masonry block vault toilet would be demolished and replaced with an ADA accessible portable toilet. Landscaping improvements including storm water rain gardens would also be made at the site.

Location

NE1/4, SE1/4, Sec. 35, T8N, R17E, Town of Oconomowoc, Waukesha County.

The existing public boat access facility is located at the end of Road 'T' between Okauchee and Upper Oconomowoc Lakes in the Town of Oconomowoc. The site is located approximately 17 miles northwest of the City of Waukesha in northwestern Waukesha County.

Background

Okauchee Lake is 1,187 acres in size and has a maximum depth of 94 feet. The lake is the third largest lake in Waukesha County. The lake has expanses of open water and many bays and coves, which makes it very attractive to boaters. This, combined with the good water quality and an excellent fishery, makes it a popular destination and one of the Departments' busiest boat launches in the southeastern region of Wisconsin. Muskie, Northern, and Largemouth Bass are common in the lake. Walleyes, pan fish and smallmouth bass can also be found in the lake. The lake is heavily developed with homes around most of the lake and is very popular with boaters especially during the summer months of May, June, July, and August.

The existing Okauchee Lake boat access site was purchased by the Department of Natural Resources in 1976, and is one of the most heavily used boat launches in the southeastern region of Wisconsin.

In 1979-80, the Department of Natural Resources developed the existing public boat launch and parking facility at the current site. The parking lot was constructed with a capacity for 17 car/trailer stalls, plus one ADA accessible car/trailer stall, one law enforcement car/trailer stall, and five car only parking stalls.

In July 2008, the Department of Natural Resources Board approved the purchase of a 0.68 acre parcel of land immediately adjacent to the south of the existing DNR boat landing facility. This parcel of land adds approximately 90 feet of lake frontage to the existing boat landing facility along the western shore of Okauchee Lake. This parcel of land was acquired using \$660,000 of State Stewardship funds with the intended purpose that it would be used for boat landing expansion

including providing additional parking, shore fishing, and improved access to Okauchee Lake. As a part of the land purchase agreement, the Department entered into a legal agreement with the Okauchee Lake Management District. This legal agreement identified how the proposed acquisition would be used in conjunction with the existing boat landing facility (see attachment 13).

A small concrete structure exists near the shoreline and was part of the dam structure at the outlet of the lake from years ago. The site contains several scattered trees, brush, and shoreline vegetation. The existing boat launch lanes consist of two 8' x 12' x 32' precast concrete panel launch lanes with a floating boarding dock between launch lanes.

An additional private boat launching facility including parking for car/trailer units can be found at the Golden Mast Restaurant on Okauchee Lake. This facility has the capacity to accommodate 13 car/trailer units.

2. Purpose and Need

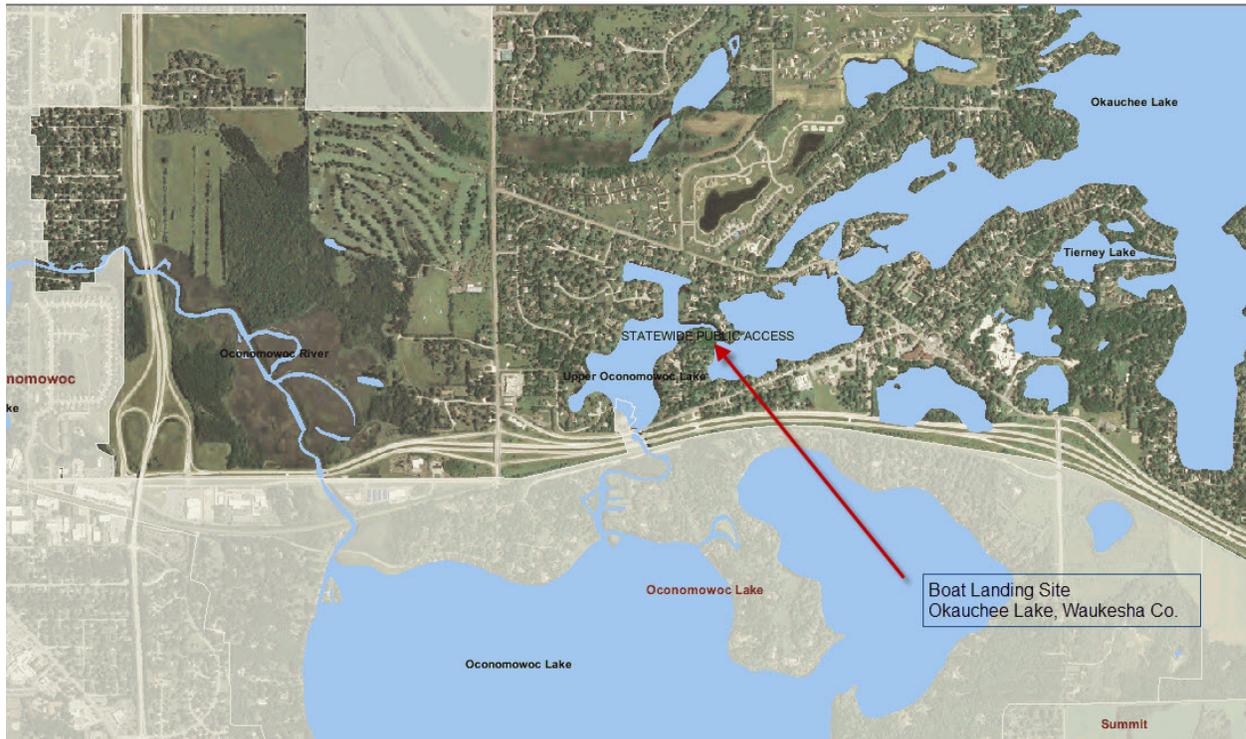
Public demand for access to waterways in southeast Wisconsin, particularly in Waukesha County, is very high. In 2008, Waukesha County had the highest number of registered boats of any county in Wisconsin. Waukesha County also has the second highest number of residents who have purchased fishing licenses. Conservation groups, anglers, and boaters in southeast Wisconsin have consistently shown strong support for public access development.

The intense popularity and significant demands for public access on Okauchee Lake and the high use of the existing public boat access site has necessitated the hiring of a private security firm to oversee boat launching and parking at the facility on weekends and holidays during the boating season. Many fights and altercations between users during peak use times have been noted in the last several years as patrons jockey to use the overcrowded boat launch facility.

Wisconsin Administrative Code NR 1.91 (Attachment 17) sets minimum and maximum standards for car/trailer parking at boat launch facilities based on the water body's size. In Okauchee Lakes' case, the standard specifies that a minimum of 29 car/trailer parking stalls and a maximum 40 car/trailer stalls need to be provided for public use on the lake.

When taking into consideration car/trailer parking at both the State and the private launch facilities, a total of 30 car/trailer stalls are available for public use on Okauchee Lake. The proposed redevelopment of the Okauchee Lake site would include a total of 23 car/trailer parking stalls. Adding the 13 car/trailer parking stalls currently available at the Golden Mast would bring the total number of car/trailer stalls available for public use to 36 upon completion of this project. This number of car/trailer stalls would fall within the acceptable minimum and maximum number of car/trailer stalls specified in NR 1.91.

The existing boat launch facility has a small concrete masonry block pit toilet. The pit toilet is old and does not meet current ADA accessibility standards for public facilities. Pit toilets built in the late 1970's or early 1980's using masonry block construction techniques typically have problems with leaking underground vaults.



Locational Map - NE1/4, SE1/4, Sec. 35, T8N, R17E, Town of Oconomowoc, Waukesha County

Description of proposed development

The proposed redevelopment of the site involves reconfiguration of the entrance road and parking lot, installation of a fishing pier, storm water control measures in the form of rain gardens and buffer strips to keep runoff from the parking lot and roads from entering the lake, removal of the pit toilet, relocation of overhead electrical lines, and placement of landscaping features.

Proposed parking includes the addition of six additional car/trailer parking stalls for a total of 23 car/trailer parking stalls at the launch facility. One ADA accessible and one law enforcement car/trailer stall would also be provided. An additional five car only stalls for a total of 10 car only parking stalls and one car only accessible parking stall would be provided at the site for fishing pier and general facility users.

One of the biggest problems with the existing boat launch facility is the current traffic flow at the site. The Okauchee public boat launch facility is currently located in a highly developed residential area at the end of a narrow dead end road, Road 'T' in the Town of Oconomowoc. All vehicles using Road 'T' to the site must either use the public boat launch facility or one of the adjacent private home owners' driveways to turn around. The fact that the road is narrow and does not allow for turning around is especially problematic for vehicles pulling trailers, when the parking lot is busy and full of vehicles. During periods of high use traffic congestion at the site increases and traffic backups are common as patrons wait to launch and retrieve boats. To alleviate traffic congestion at the site the proposed design includes a cul-de-sac at the end of Road 'T' and the entrance to the site.

The proposed design also includes the addition of an ADA accessible shore fishing pier at the site. Demand for shore fishing opportunities in southeastern Wisconsin and Waukesha County in particular, are high and in constant demand. Providing a shore fishing pier at the site would help meet this demand.

To improve water quality of runoff from the parking and roadway areas, three rain garden storm water retention areas would be constructed to collect and contain rainfall runoff from the parking lot. The site would also be landscaped to provide vegetative screenings from adjacent private homeowners.

The 35-year old pit toilet would be demolished. The pit toilet was constructed prior to many accessibility regulations for public facilities and currently does not meet many accessibility standards. In the building's place a concrete slab would be constructed to accommodate an accessible portable toilet. A natural looking board-on-board fence would be constructed to screen the proposed portable toilet from the lake.

Department funding of approximately \$33,000 has been expended on consultant design services thus far. A total of \$310,000 of boat state funds and Federal Sports Fish Restoration (SFR) grant money has been earmarked for this project. The initial SFR grant F-95-P-42 with an expiration date of June 30, 2014, has been extended to June 30, 2015. Construction is anticipated to take place in the spring of 2015, with the site being open to the public in the summer of 2015.

3. Authorities and Approvals

Wisconsin Department of Natural Resources

- Manual Code 3506.1, Permit and/or Approval Procedure; Chapters 30 and 31, Physical Alterations to Navigable Waters (see attachment 14)
- NR 216, Wisconsin Administrative Code, Stormwater Discharge.

United States Army Corps of Engineers (ACOE)

- ACOE General Permit or Letter of Permission under Section 401 and 404 of the Clean Water Act.

Local Zoning Approvals

The Department is neither required to obtain local zoning permits and approvals nor is subject to fees associated with any local authorization for projects that have a direct benefit to the general public for recreational purposes. This exemption is based on guidance provided by the Facilities and Lands Bureau Director as well as Wis. Stats. 13.48(13)(a).

The Department will be working with local zoning jurisdictions to address any concerns they may have with this proposed project.

4. Estimated Costs and Funding Source

The estimated site development cost for the Okauchee Lake site is approximately \$1.1 million dollars. An estimate of itemized costs follows:

Land Purchase (additional 0.68 ac parcel adjacent to the existing boat landing site)	\$660,000
Engineering Design Service Fees	\$40,000
Site Development including grading, paving, development of additional parking, cul-de-sac, internal roads, landscaping, etc.....	\$390,000
Fishing Pier.....	\$30,000
Total.....	\$1,120,000

5. Manipulation of Terrestrial Resources

Site work includes clearing, grubbing, grading, filling, pavement removal, hot mix asphalt paving, landscaping, and site restoration. Approximately 1,400 CY of topsoil would be stripped from the site and approximately 5,000 CY of excavation would take place.

The roadway and parking facility would be reconfigured in order to improve traffic flow and provide parking for boat launch and fishing pier users. A total of 23 car/trailer stalls plus one ADA car/trailer stall and a total of 10 car only stalls would be constructed in the reconfigured parking lot. The existing facility currently has 18 car/trailer stalls and five car only stalls.

Wisconsin Administrative Code NR1.91 sets allowable minimum and maximum numbers of parking stalls for public boat access sites. These standards are determined based on the size of the lake or water body. Okauchee Lake is approximately 1187 acres in size (ref. Wi Lakes Pub-FH-800 2005). Based on NR 1.91 standards, the minimum number of car/trailer stalls for Okauchee Lake is 29 with the maximum allowed being 40. Under the proposed design a total of 35 car trailer spaces would be provided not including one ADA disabled stall which does not count towards the NR1.91 criteria.

The new roadway and parking lot construction would require approximately 3,100 ton of aggregate placed at nine inches thickness and 5,600 SY of three inch hot-mix-asphalt. Approximately 1,500 SY of lawn area would be seeded and mulched. Landscaping would also include the planting of vegetative shrubbery in order to screen the launch site for neighboring homes and board fencing along adjacent property lines.

6. Manipulation of Aquatic Resources

No wetlands or work below the ordinary high water mark would take place at the site except for the placement of a small amount of riprap along the edges of the existing boat launch. This riprap is required to repair erosion which is taking place immediately adjacent to the concrete launch lane. In addition, a concrete shore interface would be constructed at the edge of the shoreline to accommodate the installation of a floating fishing pier at the site.

7. Buildings, Treatments Units, and Other Structures

Pit Toilet Building

The existing pit toilet building dating from 1980 would be demolished and replaced with a 20' x 12' x 8" concrete slab capable of holding a portable toilet. Three sides of the proposed concrete slab for the toilet would have a board fence to screen the view of the portable toilet from the lake. The existing pit toilet does not currently meet Americans with Disabilities Act (ADA) accessibility standards and is in poor condition. The existing building was constructed using concrete masonry units and it is believed that the vault of the building is of the same construction. It is typical of buildings using concrete masonry units for the vaults to have issues with leakage.

Kiosk

A (3) sided kiosk with 4'x4' display boards would be constructed near the boat launch lane for us in displaying boat launching and fishing regulations.

Access Road

A cul-de-sac would be constructed at the entrance of the site in order to allow vehicles which do not wish to enter the boat landing the ability to turn around at the end of Road 'T' without entering the facility. The cul-de-sac would have a radius of approximately 45 feet allowing larger vehicles and vehicles pulling trailers to turn around. The Town of Oconomowoc is in support of the creation of a cul-de-sac at this site in order to alleviate traffic congestion on Road 'T'.

Parking and Internal Roads

Approximately 340 LF x 15' of internal access roads as well as 23 car/trailer and 10 car only parking stalls would be constructed at the boat launch site. In addition, one ADA accessible car/trailer stall and one law enforcement car/trailer stall would be constructed.

Other

Approximately 350 LF of low (less than 5-ft) concrete block retaining wall would be constructed along the northwest and south property lines. Vegetative screenings would be planted and board fencing would be erected along the adjacent private property lines to act as screening on the west and south sides of the boat landing site.

8. Emissions and Discharges

Construction activity would generate temporary construction traffic, noise, dust, construction equipment engine air, emissions, and increased potential for storm water runoff.

Temporary storm water erosion controls would be installed prior to construction to prevent or minimize sediment discharge from the construction area.

Permanent rain gardens would be installed as part of this project in order to reduce runoff and total suspended solids from leaving the site. Runoff peak flow would be maintained below pre-development peak flows and total suspended solids would be reduced by more than 83% over existing conditions.

A temporary turbidity barrier would be used during construction of the concrete shore interface at the fishing pier site to prevent sediment discharge from entering the lake.

9. Other Changes

Some tree cutting and brushing would be needed in order to develop the Okauchee Lake site. Most of the cutting and brushing would take place on the parcel which was recently purchased on the south side of the existing boat launch site. The existing boat launch site is currently developed with roads and parking areas and does not need much clearing or brushing.

10. Attachments

Attachment 1 – Aerial Photo of Existing Launch Site

Attachment 2 - Aerial Photo of Existing Launch Site Plus Adjacent Lot

Attachment 3 – 100-Year FEMA Floodplain Map

Attachment 4 - Zoning Map

Attachment 5 -Topographic Map

Attachment 6 – State Historical Society Clearance

Attachment 7 – 2005 Tribal Notification Letter

Attachment 8 – 2013 Tribal Notification Letter

Attachment 9 – National Environmental Policy Act (NEPA) Documents

Attachment 10 – Federal Section 7 Evaluation

Attachment 11 – Federal Section 7 Evaluation Form

Attachment 12 – Site Specific Review

Attachment 13 – Chapter 30 Manual Code Approval of Project

Attachment 14 – 2010 Land Use Agreement with Lake Management District

Attachment 15 – Storm Water Management Report

Attachment 16 – PASER Pavement Rating

Attachment 17 – Wisconsin Administrative Code NR 1.91

Attachment 18 – Site Redevelopment Plans

Attachment 19 – Wastewater Review Comments

11. Physical Environment (topography, soils, water, air)

The Oconomowoc River flows into Okauchee Lake. Upstream of Okauchee Lake lies North Lake, Friess Lake, Little Friess Lake, Loew Lake, and Monches Impoundment. From Okauchee Lake the Oconomowoc River flows southward through Upper Oconomowoc Lake and Oconomowoc Lake, Fowler Lake, and Lac La Belle before flowing into the Rock River in eastern Jefferson County.

The existing boat landing site is predominately developed and paved for parking and boat launch maneuvering. The adjacent property which was purchased with the intent of expanding boat launch parking and to improve traffic flow through the site consists of old lawn area, trees and brush. The

home which once occupied the site was removed when the property was purchased.

12. Biological Environment (dominant aquatic and terrestrial plant and animal species and habitats including threatened/endangered resources; wetland amounts, types and hydraulic value)

Okauchee Lake is a very productive fishery with an abundance of game fish and pan fish species. The Wisconsin Department of Natural Resources fisheries staff conducted a survey of Okauchee Lake in 2002 and again in 2013-2014. The primary species targeted during spring fyke netting were northern pike, muskellunge and walleye. The primary species targeted during spring electrofishing included largemouth bass, smallmouth bass and pan fish. The results of the 2014 survey revealed that Okauchee Lake is a robust fishery having abundant and healthy populations of northern pike, largemouth bass, black crappies and bluegills. The northern pike catch rates during spring fyke netting revealed excellent catch rates and moderate size structure. Largemouth bass catch rates were high, indicating above average abundance and excellent size structure. Black crappie size structure and abundance is excellent in Okauchee Lake providing excellent angling opportunity for this popular pan fish species. Muskellunge catch rates are low but size structure is excellent having an average size of over 40 inches. Walleye are found in very low abundance as indicated by both spring fyke netting and electrofishing catch rates. Other fish found to inhabit Okauchee Lake include long nose garfish, green sunfish, pumpkinseed, rock bass, yellow perch, yellow bullhead, black bullhead, brown bullhead, cisco, golden shiners, brook silversides and common carp.

Invasive species found in Okauchee Lake include the following; Banded Mystery Snail, Chinese Mystery Snail, Curly-Leaf Pondweed, Eurasian Water-Milfoil, Freshwater Jellyfish, Hybrid Eurasian / Northern Water-Milfoil, Purple Loosestrife and Zebra Mussels.

Land surrounding the Okauchee Lake public boat access site is considered urban and is developed with lakefront homes. Common mammals using the lake include muskrats, mink, raccoons, and little brown bats. Other mammals in surrounding areas include red, gray, and fox squirrels, cotton tail rabbits, red and gray fox, coyotes, skunks, opossums, woodchucks, eastern chipmunks, thirteen-lined ground squirrels, and various species of shrews, moles, mice, voles, and weasels.

Common birds on the lake include Canada geese, mallards, wood ducks, blue-winged teal, American coots, belted kingfishers, herring gulls, ring-billed gull, common egrets, great blue herons, and various species of swallows, shorebirds, and song birds. Other waterfowl which may use the lake for spring or fall migratory stop-overs include, widgeon, green-winged teal, northern pintail, gadwall, northern shoveler, bufflehead, common goldeneye, ring necked duck, hooded merganser, lesser scaup, and other duck species. Birds found on surrounding wetlands and uplands include American bitterns, sandhill cranes, woodcock, owls (great horned, screech, and barred), hawks (red-tailed, Coopers, sharp-shinned, and American kestrel), wild turkeys, and a large variety of songbirds. Bird feeders are common at residences along the shoreline, attracting cardinals, gold finches, nuthatches, chickadees, blue jays, mourning doves, downy woodpeckers, and other species.

Common reptiles and amphibians around the lake include painted and snapping turtles, eastern garter snakes, fox snakes, milk snakes, common garter snakes, northern red belly and northern water snakes, American toads, spring peepers, gray tree frogs, leopard frogs, wood frogs, green frogs, and chorus frogs. Other reptile and amphibian species are also likely to be present.

No state or federal threatened or endangered species or state species of special concern have been observed at the existing site. The Department reviewed the Natural Heritage Inventory (NHI) file for

the project area. Species recorded within five miles of the project area include: American eel (*Anguilla rostrata*), lake chubsucker (*Erimyzon sucetta*), least darter (*Etheostoma microperca*), pugnose shiner (*Notropis anogenus*), slender madtom (*Noturus exilis*), and salamander mussel (*Simpsonia ambigua*). Suitable habitat for each of these species does not exist at the proposed project site. No wetlands exist at the existing boat launch site nor the adjacent property which was acquired in 2010.

13. Cultural Environment

Land use

Most of the land around Okauchee Lake is privately owned and contains single family homes. Okauchee Lake lies within the Towns of Oconomowoc and Merton. Most of the property around the lake is zoned for low density residential development with some environmental corridor. Most lakeshore property owners have piers for docking and own their own personal watercraft. The lake is primarily used for fishing and pleasure boating.

A portion of the shoreline of the existing launch site and adjacent property lies on the edge of the 100-year floodplain according to the Waukesha County GIS maps.

The existing project site is located in an urban area of the Town of Oconomowoc. Adjacent private residences are located to the south and southwest of the Department site as well as across the channel connecting upper Oconomowoc and Okauchee Lakes North of the Department site.

The site is currently zoned P-1 and R-3 based on Waukesha County zoning maps.

Social/Economic

According to U.S. Census data for 2010, Waukesha County held a population of 389,891. The County's population increased 29,124 from 2000 to 2010 which represents an 8.1% growth rate. Waukesha County has the third highest county population in the state.

Public demand and the expectation for public access to waterways in southeast Wisconsin, particularly in Waukesha County, is very high. In 2008, Waukesha County had the highest number of registered boats of any county in Wisconsin, and had the second highest number of resident fishing licenses. Conservation groups, anglers, and boaters in southeast Wisconsin have consistently shown strong support for public access and are very supportive of public access to Okauchee Lake. It is common for the existing boat launch site to fill to capacity early in the day and for all of the parking stalls to be occupied for most of the day.

A security guard is employed every weekend from opening day of fishing season to Labor Day to provide security and to maintain order at the boat launch site. Additional personnel are employed on holidays.

Archaeological/Historical

The Department Archeologist and the State Historic Preservation Officer have both reviewed the site in 2013, and have determined that the site does not have historic or archeological significance.

The Department provided tribes notice of intent to develop the proposed site in 2005, pursuant to Section 106 of the national Historic Preservation Act. The Department did not receive any responses from any tribes for this project. Lack of response is therefore considered presumptive approval.

14. Other Special Resources (e.g. State Natural Areas, prime agricultural lands)

None.

ENVIRONMENTAL CONSEQUENCES (probable adverse and beneficial impacts including indirect and secondary impacts)

Several environmental staff from the Department were consulted with respect to the environmental effects and concerns that this project may create.

No negative comments were provided by Department experts.

This project was reviewed under authority of Wis. Stats. Chapter 30, by water regulation staff of the Department of Natural Resources. A Manual Code approval is required for work taking place below the ordinary high water mark of the lake, namely the placement of riprap at the boat launch lane and placement of the public fishing pier.

Several comments were received from a Department wastewater specialist. These comments can be found in the Attachment 19. These comments were forwarded to the design consultant and are being incorporated into the final set of design plans. A Notice of Intent (NOI) storm water permit is required from the Department prior to commencement of work on this project.

Only positive comments were received from the Department's fisheries and water quality staff with respect to the water quality benefits of installing rain gardens and buffer strips. Storm water runoff currently flows directly into the lake and there is very little opportunity for buffer strips between the existing parking lot and the lake.

15. Physical (include visual if applicable)

The proposed access road and parking lot development have been designed to permanently control suspended solids and peak runoff from the site in order to protect water quality at the site as shown by the Storm Water Management Report Attachment 15.

The plans call for vegetative screenings and board-on-board fencing along the property lines in order to minimize visual impacts of the site on adjacent land owners. In addition, board fencing would be constructed around three sides of the portable toilet concrete slab in order to screen the toilet from the lake.

Several trees and shrubs would be removed from the proposed site as the result of redeveloping the project site. The site would become more visually apparent from the lake as the result of this clearing and grubbing.

Placement of the fishing pier may displace boaters in the immediate vicinity of the fishing pier and would result in effectively removing a small portion of the lake watercraft navigation.

16. Biological (including impacts to threatened/endangered resources)

The proposed expansion of the Okauchee Lake boat launch would have minimal negative impacts to the physical fisheries habitat because there is no critically sensitive habitat found in the project area. The proposed Okauchee Lake boat launch would benefit fisheries habitat with the addition of buffer strips and rain gardens which would reduce run off and nutrient loading ultimately contributing to improved water quality.

No impacts to threatened or endangered resources are anticipated with this project because of the lack of habitat in the project area. The federal Fish and Wildlife Service expressed some concerns over the removal of trees and the possibility of this impacting the endangered northern long eared bat. Upon further review, it was decided that the limited number of trees anticipated to be cut at the site may result in the incidental take of bats, but the numbers of bats affected would be low. Effects on the reproduction and numbers of bats would be localized and therefore not cause any negative effects on species distribution.

Several invasive species are currently found in Okauchee Lake. Riparian and transient boating and recreation activities currently exist at the site and would continue to exist creating the potential for invasive species introduction even if this project is not undertaken.

17. Cultural

a. *Land Use (including indirect and secondary impacts)*

Much of the current site is used for boat launching and parking car/trailer units and its land use would not change. An adjacent vacant lot which used to have a residential home would be developed with additional boat launching infrastructure including a road, and room for parking and a fishing pier. The private residence including a well and sewer system were demolished in approximately 2009, with the intent to expand the current boat launch facility onto the added lot. The adjacent property is currently not maintained and is overgrown with scattered trees and brush.

Department site activity and secondary noise impacts would be limited to boat launching and retrieval, associated vehicle parking, fishing pier use, and site maintenance during hours of operation. The site may be used to provide access for winter recreation, but the nature of the channel and current in close proximity to the boat launch may make winter access of the lake difficult and unsafe the majority of the time. Better access to the lake in winter from the most southern side of the project site may still be possible. Picnicking, swimming, camping, and hunting would not be allowed at the site.

The Department would work with the local lake management district to post hours of operation and other information at the site, and to control site access. Department public access sites are typically open 24-hours a day. NR 1.91(4)(d)6, Wis. Adm. Code, states that any boat launched during operation hours must be allowed egress from the water at any time.

The proposed improvements to the site would allow security personnel to station themselves at the entrance to the site in order to control entrance when all parking spaces are occupied and would provide a good line of sight to the existing launching lanes in order to monitor boat retrieval and launching.

Department staff would inspect, manage, and maintain the Department site. As stewards of public property, the Department would address site-specific issues. The portable toilet would be provided and maintained by a private contractor. If there is no demand for a year-round portable toilet facility at the site, it would be removed in the winter.

Car-trailer traffic would continue to use Road 'T' to access the boat launch site. The proposed cul-de-sac should help alleviate traffic congestion by allowing patrons pulling car/trailer units to turn around without having to use private driveways and without having to enter an already crowded boat launching facility in order to seek other launch locations during times of high use. Traffic back-ups on Road 'T' in front of the many private residences along the roadway should be reduced.

b. Social/Economic (including ethnic and cultural groups, and zoning if applicable)

It is anticipated that this project will not have any negative impacts on ethnic or cultural groups.

Based on the Department's experience with other public access sites in Waukesha County, the types of boats using public boat ramps include: gasoline powered speed and pleasure boats, fishing boats, canoes, kayaks, sailboats, and personal watercraft such as jet skis. It is anticipated that sailboat use at this site will remain very small due to the small body of water accessible from the current launch site. These watercraft types are currently common on Okauchee Lake. Many riparian property owners typically own one or more watercraft for recreational use on the Lake.

The Department anticipates little impact on watercraft operations and safety on the lake. Most impacts would occur on summer weekends and holidays. Improvements to the existing facility should make traffic and boat launching and retrieval flow smoother than with the existing facility. It is anticipated that the security personnel who currently oversee the site on weekends would still provide security to the site once the improvements are completed. Possible conflicts between watercraft users and fishers using the fishing pier may occur. The fishing pier is proposed to be located as far from the launch lanes as possible in order to minimize the possibility of conflicts.

All transient and riparian boaters are required to know and comply with state and local boating regulations. State regulations require personal watercraft to travel at slow no-wake speeds within 200-feet of the shoreline of any lake, and at slow no-wake speeds within 100 feet of another watercraft.

State boating laws apply to all inland lakes in Wisconsin. Local municipalities have the authority to pass ordinances more restrictive than state law. Chapter 30.77, Wisconsin statutes, allows local municipalities to enact local regulations relative to boating in the interest of public health, safety, or welfare, including the public's interest in preserving the state's natural resources. Typical local regulations address water-skiing, shore land zone protection, slow no-wake hours and areas, and speed limits. Several municipalities in southeastern Wisconsin have passed ordinances prohibiting the use of cars, trucks, all-terrain vehicles, and snowmobiles on ice covered lakes. Local municipalities would be allowed to post local ordinances at the site.

c. Archaeological/Historical

No archaeological concerns were noted by the State Archaeologist or the State Historic Preservation Officer.

18. Other Special Resources (e.g. State Natural Areas, prim agricultural lands)

No other special resources would be impacted by this project.

19. Summary of Adverse Impacts that cannot be avoided

- Several mature trees and shrubs would be removed and the vacant lot adjacent to the existing site would be developed.
- The appearance of the site would change.
- Dusk to dawn lighting would exist at the launch.
- Wildlife and aquatic habitat, however small, in the immediate area of work may change.
- Wildlife, including the possibility of the northern long eared bat, may be displaced.
- Impervious surfaces due to the parking lot and access roads would increase.
- Temporary construction impacts would occur, including: construction noise, fugitive dust, erosion, and construction equipment exhaust.
- The facility would close for several months during construction at the site. It is anticipated that construction would take place in the spring of 2015.
- Traffic on Road 'T' may increase slightly once the new facility is completed but traffic flow should be more efficient.
- Boat launching and retrieval, fishing pier use, and vehicle parking noise and activities at the site would continue.

DNR EVALUATION

20. Duration of Environmental Effects

a. Long-term versus short-term environmental effects

By maintaining the boat access facility, the Department would be providing long term recreational opportunities to the citizens of the state and economic benefits to local businesses in the area.

Short term effects would include locally increased traffic, noise, dust, exhaust, and erosion during construction of the new facility.

Construction of the new boat launching facility including roadway, parking lot, and storm water management practices including the removal of existing trees and shrubs, grading, and landscaping would permanently change the visual appearance of the site.

Wildlife may be temporarily displaced during construction of the project. There would be a permanent decrease in locally available habitat. Several trees and shrubs would be removed in order to create additional roadway and parking at the site.

Boat launching and retrieval, use of the fishing pier, and associated vehicle traffic and parking activity would be permanent activities. The 23 car/trailer parking stalls and 10 car only parking stalls excluding required ADA parking spaces would result in minimal permanent increases in car/trailer traffic, watercraft traffic, boat wakes, shoreline erosion, and possible conflicts between watercraft

users and fishers. Secondary noise and traffic impacts would likely peak during summer weekends and holidays.

b. *Effects on geographically scarce resources (e.g. historic or cultural resources, scenic and recreational resources, prime agricultural lands, threatened or endangered resources or ecologically sensitive areas)*

None.

c. *Reversibility of environmental effects*

The proposed boat launch expansion would be a permanent development. Removal of the facility is possible but unlikely.

21. Cumulative Effects

The site's appearance and level of activity related to roadway traffic, boat launching and retrieval, and vehicle parking would be consistent with the Department's public boat launches on other lakes in the southeastern part of the state. The 23 car/trailer parking stalls would result in minimal permanent increases in watercraft traffic, boat wake, and shoreline erosion.

The Department intends to follow best management practices throughout construction in order to minimize erosion.

The proposed rain gardens and buffer strips would reduce peak runoff and total suspended solids (TSS) from the parking lot and road surfaces. The peak runoff from the site is anticipated to be reduced to below pre-construction conditions and total suspended solids (TSS) would be reduced by over 83%.

Cumulative impacts on wildlife, fish, water quality, or lake habitat are not anticipated.

The acquisition and development of additional public access sites throughout southeastern Wisconsin would address the growing public expectation for greater recreational access to public waters. This may help to moderate the intensive use of other public access sites in the area.

22. Risk

a. *Unknowns that create substantial uncertainty in predicting effects on the quality of the environment.*

None.

b. Operating problems such as malfunctions, spills, fires or other hazards (particularly those relating to health or safety).

Emergencies, fires, and spills at the Department site could be reported by calling 911. Department conservation wardens and local law enforcement personnel would provide emergency response services.

One car/trailer parking stall for only Department and law enforcement personnel would be designated at this site giving first responders guaranteed parking and access for their equipment in the event of a water emergency.

Incidental petroleum spills at such sites are typically less than five gallons.

23. Precedence

Redevelopment of the Okauchee Lake site would not set a precedent on future access site developments in southeastern Wisconsin.

24. Controversy

The planned redevelopment of the Okauchee Lake site is consistent with the public's expectation that the Department provide year-round public access to recreational boaters and anglers to Okauchee Lake.

As with all public boat access projects, there are some residents who do not support the Department's goal of providing free public access to the waters of the state. Local concerns typically include changes in site appearance, increases in traffic, public activity, project costs, public benefits, introduction of invasive species, and increased levels of crime.

The Town of Oconomowoc is supportive of this project especially with respect to providing a cul-de-sac in order to reduce traffic congestion on Road 'T'.

ALTERNATIVES

25. No action and other alternatives

a. Alternative A – Do Nothing

This alternative would not address the poor condition of the existing parking lot and roadway surfaces, erosion next to the boat launch, standing water in the parking lot, traffic congestion at the boat launch and on the town Road 'T' leading to the site, and runoff from existing parking surfaces. Users would continue to access and use the existing boat launch site and to turn around when the parking lot is full. Doing nothing would also do nothing to address the high demand for public access on Okauchee Lake.

The existing parking and road surfaces are showing their age and are in need of repair or replacement. Doing nothing would not address the current deteriorating condition of the road and parking surfaces.

b. Alternative B – Resurface Existing Parking Surfaces Only

This alternative would not address the traffic congestion concerns, runoff from existing impervious surfaces, or the high demand for public access on Okauchee Lake. Construction would likely be less disruptive than for the proposed project.

SUMMARY OF ISSUE IDENTIFICATION ACTIVITIES

26. Agencies, citizen groups and individuals contacted

August 6, 2013 – DNR meets with immediate adjacent property owners (Jim Tesmer, GERALYN McGEE, Ed Hicks)

Benjamin Heussner – DNR Fisheries Biologist, Eagle Wisconsin

Geri Radermacher – DNR Water Regulation and Zoning Specialist, Waukesha

Heidi Bunk – DNR, Water Resources Management Specialist, Waukesha

Maureen McBroom – DNR Wastewater Specialist, Milwaukee

Douglas Zeihen – DNR Conservation Warden, Waukesha

Lynette Check – DNR Natural Resources Engineer, Milwaukee

Lance Stock – Parks and Recreation Specialist, Lapham Peak

Brian Hefty – Natural Resources Property Supervisor, Dodgeville

NOTE: GAP EXISTING
IN LEGAL DESCRIPTIONS



Attachment 2



NOTE: GAP EXISTING
IN LEGAL DESCRIPTIONS

OKRIDGE
LAKE
AREA

**Purchased
Property**

ROAD T

0572 988

0572 970

0572 001

0572 003

0572 002

1

2

84

83

0569 19

0572
990

0569 192

0569 191

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

0569 19

I want to...

Public L

Attachment 5



5

STATEWIDE PUBLIC ACCESS

Site

Okauchee Lake

Oconomowac Lake Upper

Bay 3/1/08 Ct

Hillcrest Ct

Lakeview Ct

Rivier Dr

Lakeland Rd

Lakeland Rd

Shady Ln

Comfort Dr

Lacey Ln

16

250ft
100m

RECEIVED
JUN 13 2013
FACILITIES AND LANDS

HP-05-07 (8/15/03)

For SHPO Use Only. Case #

13-0459/WK

REQUEST FOR SHPO COMMENT AND CONSULTATION ON A FEDERAL UNDERTAKING

Submit one copy with each undertaking for which our comment is requested. Please print or type. Return to:

Wisconsin Historical Society, Division of Historic Preservation, Office of Preservation Planning, 816 State Street, Madison, WI 53706

Please Check All Boxes and Include All of the Following Information, as Applicable:

RECEIVED
MAY 23 2013
DIV HIST PRES

I. GENERAL INFORMATION

- This is a new submittal.
- This is supplemental information relating to Case #: _____ and title: _____
- This project is being undertaken pursuant to the terms and conditions of a programmatic or other interagency agreement. The title of the agreement is _____

- a. Federal Agency Jurisdiction (Agency providing funds, assistance, license, permit): USFWS (SFR)
- b. Federal Agency Contact Person: MARK DUDZIK Phone: 608.266.3426
- c. Project Contact Person: MARK DUDZIK Phone: 608.266.3426
- d. Return Address: WI DNR 100 S. WEBSTER, MADISON, WI Zip Code: 53707-7921
- e. Email Address: mdudzik@wi.gov
- f. Project Name: OKAUCHEE LAKE BOAT ACCESS
- g. Project Street Address: _____
- h. County: WK City: Okauchee Zip Code: _____
- i. Project Location: Township 8N, Range 17E, E/W (circle one), Section 35, Quarter Sections NW/SE
- j. Project Narrative Description—Attach Information as Necessary.
- k. Area of Potential Effect (APE). Attach Copy of U.S.G.S. 7.5 Minute Topographic Quadrangle Showing APE.

II. IDENTIFICATION OF HISTORIC PROPERTIES

- Historic Properties are located within the project APE per 36 CFR 800.4. Attach supporting materials.
- Historic Properties are not located within the project APE per 36 CFR 800.4. Attach supporting materials.

III. FINDINGS

- No historic properties will be affected (i.e., none is present or there are historic properties present but the project will have no effect upon them). Attach necessary documentation, as described at 36 CFR 800.11.
- The proposed undertaking will have no adverse effect on one or more historic properties located within the project APE under 36 CFR 800.5. Attach necessary documentation, as described at 36 CFR 800.11.
- The proposed undertaking will result in an adverse effect to one or more historic properties and the applicant, or other federally authorized representative, will consult with the SHPO and other consulting parties to resolve the adverse effect per 36 CFR 800.6. Attach necessary documentation, as described at 36 CFR 800.11, with a proposed plan to resolve adverse effect(s).

Authorized Signature: _____ Date: 05.21.2013

Type or print name: MARK DUDZIK, DNR

IV. STATE HISTORIC PRESERVATION OFFICE COMMENTS

- Agree with the finding in section III above.
- Object to the finding for reasons indicated in attached letter.
- Cannot review until information is sent as follows: _____

Authorized Signature: DAN DUCHROW Date: 6/10/13



State of Wisconsin \ DEPARTMENT OF NATURAL RESOURCES

Jim Doyle, Governor
Scott Hassett, Secretary

101 S. Webster St.
Box 7921
Madison, Wisconsin 53707-7921
Telephone 608-266-2621
FAX 608-267-3579
TTY Access via relay - 711

February 16, 2005

Ms. Barbara Kyser-Collier
Environmental Director
Wyandotte Nation
P.O. Box 250
Wyandotte, OK 74370

Dear: Ms. Kyser-Collier:

In conformation with Section 106 of the National Historical Preservation Act, the Wisconsin Department of Natural Resources gives notice that it intends to use federal funds as follows:

- A. Applicant Agency: Wisconsin Department of Natural Resources
P.O. Box 7921
Madison, WI 53707
- B. Environmental Impact Statement Required: (No)
- C. Federal Program and Agency: Federal Aid in Sport Fish Restoration
U.S. Fish and Wildlife Service
- D. Estimated Date of Commencing Work: Summer of 2006
- E. Project Description: Renovation of the Okauchee Lake Public Boat Access Site
- F. Legal Description: T08N,R17E, NW 1/4, SE 1/4 SEC 35

If you are aware of any traditional cultural properties or sacred sites in the vicinity of this project, please let us know. If we do not receive a response from you within 30 days, we will assume you have no comments regarding this project. If you have any questions, please contact me at (414) 263-8690.

Sincerely,

A handwritten signature in black ink that reads 'Lynette Check'. The signature is fluid and cursive.

Lynette Check
SER Engineer



February 3, 2014

Tribal Historic Preservation Officers
State of Wisconsin
Via E-mail

Subject: WDNR Proposed Public Boat Access Development – Okauchee Lake, Waukesha County, Wisconsin

Dear Tribal Historic Preservation Officer:

In conformance with Section 106 of the National Historic Preservation Act, the Wisconsin Department of Natural Resources gives notice that it intends to use federal funds as follows:

- A. Applicant Agency: Wisconsin Department of Natural Resources
P.O. Box 7921
Madison, WI 53707
- B. Environmental Impact Statement Required: No.
- C. Federal Program and Agency: Federal Aids in Sport Fish Restoration
U.S. Fish and Wildlife Service
- D. Estimated Date of Application or Commencing Work: 90 days from date typed above.
- E. Project Description: The scope of the project will entail renovating the parking lot on include additional parking at the existing boat access site on Okauchee Lake and construction of a public fishing pier. Work will include: grading and paving, construction of rain gardens for storm water retention, demolition of an existing pit toilet, and construction and installation of a new public fishing pier.
- F. Legal Description: Sec. SE1/4, Sec 35, T8N, R17E, Town of Oconomowoc, Waukesha County (See attached map).

When responding to this letter, please reference the information cited on the subject line. If we don't receive a response from you within 30-days, we will assume you have no comments regarding this project.

If you have any questions, please contact me at 608-267-7484.

Sincerely,

John M. Olson, P.E.
State Boat Access Engineer
Bureau of Facilities and Lands

NEPA COMPLIANCE CHECKLIST

State: Wisconsin Federal Financial Assistance Grant/Agreement/Amendment Number: F-95-P-21
Grant/Project Name: Okauchee Lake Boat Access Development. This project is covered under an EA and FONSI dated 7/29/04 for boat access development and renovation sites in Wisconsin

This proposal is not completely covered by categorical exclusion 1.4B (a) 516 DM 2, Appendix 1; and/or 516 DM 6, Appendix 1.

(check (✓) one) (Review proposed activities. An appropriate categorical exclusion must be identified before completing the remainder of the Checklist. If a categorical exclusion cannot be identified, or the proposal cannot meet the qualifying criteria in the categorical exclusion, or an extraordinary circumstance applies (see below), an EA must be prepared.)

Extraordinary Circumstances:

Will This Proposal (check (✓) yes or no for each item below):

- List of 12 extraordinary circumstances with checkboxes. All 'No' boxes are checked.

(If any of the above extraordinary circumstances receive a "Yes" check (✓), an EA must be prepared.)

This grant/project includes additional information supporting the Checklist.

Concurrences/Approvals

Project Leader: [Signature]

Date: 4/13/05

State Authority Concurrence: [Signature]

Date: 4/20/05

(with financial assistance signature authority, if applicable)

Within the spirit and intent of the Council of Environmental Quality's regulations for implementing the National Environmental Policy Act (NEPA) and other statutes, orders, and policies that protect fish and wildlife resources, I have established the following administrative record and have determined that the grant/agreement/amendment:

- is a categorical exclusion as provided by 516 DM 6, Appendix 1 and/or 516 DM 2, Appendix 1. No further NEPA documentation will therefore be made.
is not completely covered by the categorical exclusion as provided by 516 DM 6, Appendix 1 and/or 516 DM 2, Appendix 1. An EA must be prepared.

Service signature approval:

RO or WO Environmental Coordinator: _____ Date: _____

Staff Specialist, Division of Federal Assistance: _____ Date: _____

(or authorized Service representative with financial assistance signature authority)

REGION 3 FEDERAL ASSISTANCE SECTION 7 EVALUATION FORM

PHASE I: COMPLETED BY STATE

(See Phase I Instructions for completing this form)

For federal assistance programs administered by the USFWS (Division of Federal Assistance.)

Wisconsin

Wisconsin DNR

State: _____ **Agency:** _____

Grant Program(s): Sport Fish Restoration

Grant Proposal (GP), Grant Agreement (GA), GP Amendment, GA Amendment (Circle all that apply)

Title and Number (add amendment no.): Okauchee Lake Boat Access Renovation F-95-P-31

I. Species/Critical Habitat: List species or critical habitat (or attach list) that are and/or may be present within the action area. **A review of ER/NHI data revealed that there are no Federally protected species or critical habitat in the vicinity of this project.**

II. Description of Proposed Action: Describe the action(s) in sufficient detail so that the potential effects of the action can be identified and fully evaluated. This project will renovate the public boat access site on Okauchee Lake in Waukesha County. This project will add new parking space to the landing site, expand the existing ramp and add accessible parking and boarding dock. This project is located in a rapidly growing part of southeast Wisconsin and will provide added access to a popular lake in this region.

III. Description of Effects: Describe the effects, including beneficial, of the project actions on Species/Critical Habitat. **A review of ER/NHI data revealed that there are no Federally protected species or critical habitat in the vicinity of this project.**

IV. Recommended Determination(s) of Effect(s): For all species and critical habitat identified in Section I, mark (X) the appropriate determinations.

A. Listed, Proposed and Candidate Species

a) "No Effect"

List species for which this recommendation is applicable (or attach list):

b) "May Affect, but is Not Likely to Adversely Affect"

List species for which this recommendation is applicable (or attach list):

c) "May Affect, and is Likely to Adversely Affect"

List species for which this recommendation is applicable (or attach list):

B. Designated and Proposed Critical Habitat

a) "No Effect" to Critical Habitat

List critical habitat(s) for which the recommendation is applicable. _____

List critical habitat(s) for which the recommendation is applicable. _____

___ b) *“May Affect, but is not likely to Adversely Affect”*
List critical habitat(s) for which the recommendation is applied. _____

___ c) *“May Affect, and is Likely to Adversely Affect”*
List critical habitat(s) for which the recommendation is applied. _____

State Signatures:

Prepared by:

Name/Title: Dave Daniels, WDNR SFR Grant Project Staff

Signature: *Dave Daniels* Date: 3-26-07
Telephone No. 715-365-8914 email: david.daniels@wisconsin.gov

Reviewed by:

Name/Title: RANDLE L. JUREWICZ

Signature: *Randle L. Jurewicz* Date: 3/28/07
Telephone No. _____ email: _____

REGION 3 FEDERAL ASSISTANCE SECTION 7 EVALUATION FORM

PHASE 3 Part A: Completed by Ecological Services Field Office

Grant Proposal/Agreement/Amendment Title and Number: _____

Listed Species

I. Programmatic Recovery Biological Opinion: _____ **Date Issued:** _____

II. Actions identified on the attached Phase 1 Form were contemplated in the referenced above Biological Opinion. Yes _____ No _____ .

III. The appropriate conservations measures identified in the referenced above Biological Opinion have been explicitly incorporated into the project design and are described in the attached Phase 1 Form. Yes _____ No _____ .

IV. The anticipated effects of the proposed action as described on Phase 1 Form are commensurate with the effects anticipated in the referenced above Biological Opinion. Yes _____ No _____ .

V. Anticipated Take. There is sufficient information available about the proposed action to determine the amount and extent of incidental take. Yes _____ No* _____ .

If Yes, complete sections 1 and 2 below:

1. Describe the type & extent of take anticipated to occur as a result of the proposed action.

2. Reconcile take anticipated with proposed action with the type & extent of take authorized via the referenced above Biological Opinion (describe take authorization provided in the programmatic and confirm that the level anticipated with the proposed action is within those specified limits).

*If there is not sufficient information available to complete this section at the grant agreement/proposal stage, then a future project-specific section 7 consultation is required. States will provide project-specific information to the ESFO as project information becomes available. Incidental take anticipated to result from the proposed action will be described during the project-specific consultation and will be documented on a Phase 3B form. The Phase 3B form will also describe the reasonable and prudent measures that must be followed to exempt the incidental take.

VI. The appropriate RPMs and TCs identified in the referenced above Biological Opinion have been explicitly incorporated into the project design and are described on the Phase 1 Form. Yes _____ No _____ .

Proposed & Candidate Species

I. Species: _____

II. The proposed action as described on the attached Phase I form is Likely to result in Jeopardy or destroy or adversely modify critical habitat (provide rationale for conclusion either in space below on a separate sheet of paper).

Yes _____ No _____ .

Conclusion

This concludes Section 7 consultation of the proposed action _____ .
Formal conference is required for proposed/candidate species _____ .
Further section 7 review is required at the project level (Phase 3B form required) _____ .

Reviewing Biologist

Date

Appendix C - Site Specific Review

Okauchee Boat Access Renovation Project

Issue	Trigger	Site Status	Issue Triggered
Listed Species	A "likely to adversely affect" designation would indicate the need for further consultation with USFWS to determine if a site-specific document is needed.	ER/NHI Screening showed no Federally protected species or critical habitat present. Section 7 Dated: 4/20/05	No
Cultural Resources	If significant cultural resources exist that cannot be addressed to SHPO satisfaction, USFWS will be consulted concerning the need for a site-specific EA.	SHPO noted that the project may proceed on 2/16/05.	No
Floodplains	If major floodplain problems exist that cannot be addressed, USFWS will be consulted for advice and guidance concerning the need for a site-specific EA.	There will be no meaningful change to the floodplain as a result of this project.	No
Wetlands	If net loss of wetlands exceeds 5 acres or if any net loss of wetlands that can't be adequately mitigated is likely to result from an access development project, USFWS will be consulted on the need for a site-specific EA.	There will be no negative impact or loss to wetlands as a result of this project.	No
Invasive Species	Should it be shown that providing access may pose an unreasonable risk of introducing invasive species to sensitive areas, USFWS will be consulted on the need for a site-specific EA.	This project is not in a sensitive area and will not cause a major risk of introducing additional species	No
Environmental Justice	If situations arise that indicate a possible adverse effect on minorities or low-income individuals, USFWS will be consulted on the need for a site-specific EA.	This project will be a positive benefit in the community for low income and minority populations in southeast Wisconsin.	No
Public Involvement	If public controversy persists, USFWS will be consulted on the need for a site-specific EA.	Following community notification of this project there has not been significant negative public comment.	No
No Access Waterway	If the rare situation arises where it is possible that this could be an issue, USFWS will be consulted on the need for a site-specific EA.	This project will improve and modernize an existing private landing and bring it up to state and federal standards,	No
Cumulative Impacts	If cumulative impacts are determined to be a possible issue, USFWS will be consulted on the need for a site-specific EA.	No problems associated with this project have been identified at this site.	No
Traffic Flow and Safety	If construction of the site would cause problems to existing traffic flow or create safety problems with regard to vehicle or boat traffic, USFWS will be consulted on the need for a site-specific EA.	Improved traffic flow and visitor safety to the boat access site will be seen as a direct result of this project.	No

CORRESPONDENCE/MEMORANDUM

DATE: April 8, 2014 FILE REF: IP-SE-2014-68-00470

TO: Lynette Check, DNR
Lance Stock, DNR

FROM: Geri Radermacher

SUBJECT: Okauchee Lake Public Access Improvements

Approval of Department Project located in or adjacent to navigable waters.

Activity Description, Project Description and Location: The DNR c/o Lynette Check and Lance Stock proposes improvements to the existing Okauchee Lake Public Access site. The overall project involves replacing existing asphalt and expanding the parking area, adding an ADA fishing pier and an ADA toilet, relocating a dry hydrant, top dressing riprap and constructing a rain garden. The Chapter 30 authority on the project is limited to the placement of the fishing pier, the installation of the dry hydrant and the placement of riprap. The project does not include the repair or replacement of the existing boat ramp or loading pier. The DNR's stormwater program has authority on the grading activities proposed at the site, including, but not limited to, construction site erosion control and post construction stormwater practices, The project is located NW1/4-SE1/4 S35, T8N, R17E, Town of Oconomowoc, Waukesha County.

Sponsor: DNR, Lynette Check and Lance Stock

The project has been reviewed and found to be consistent with the standards of:
30.12 Wis. Stats. and Chapters NR 102, 103, 150, 299, and 500 through 520 of the Wisconsin Administrative Code.

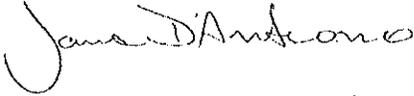
Conditions of Approval:

1. This approval authorizes the placement of the concrete abutment associated with the proposed fishing pier. Prior to starting the project, you shall submit a construction detail as well as an erosion control plan, dewatering plan and construction sequence for the concrete abutment. These plans shall be reviewed and approved by Geri Radermacher prior to starting any phase of the project.
2. This approval authorizes the placement of the fishing pier at the location of the approved concrete abutment. Final pier plans shall submitted to and approved by Geri Radermacher prior to placement. Plans shall include items such as material, length, and width.
3. This approval authorizes the placement of a dry hydrant. Prior to starting the project, you shall submit a construction detail as well as an erosion control plan, dewatering plan and construction sequence for the dry hydrant. These plans shall be reviewed and approved by Geri Radermacher prior to starting any phase of the project.
4. This approval authorizes the "top dressing" of existing riprap located adjacent to the boat ramp. You shall submit details of the riprap which include type of rock, where the rock will be placed, slope of the proposed rock, height of the proposed rock. Riprap shall be circular natural field stone 6-24 inches in diameter and shall be placed at a 2:1 slope. These plans shall be reviewed and approved by Geri Radermacher prior to starting any phase of the project.
5. *You shall hold a preconstruction meeting in which all contractors and subcontractors and a representative of the DNR WRZ and/or Stormwater program attend. You shall notify both Geri Radermacher and Stormwater Specialist Maureen McBroom of the date and time of the preconstruction meeting a minimum of seven working days prior to the meeting.*

All necessary approvals shall be obtained from the Waukesha County, the Town of Oconomowoc, the U.S. Army Corps of Engineers and the DNR Stormwater Program prior to the start of the project.

The below approval authorizes this Department of Natural Resources project according to Manual Code 3565.1.

APPROVED:



James D'Antuono
Waterways Field Supervisor

April 8, 2014

cc: Maureen McBroom, DNR
Waukesha County Parks and Land Use
Marie Kopka, ACOE

file

WHEELER, VAN SICKLE & ANDERSON, S.C.
a Wisconsin Service Corporation

ATTORNEYS AT LAW

SUITE 801
25 WEST MAIN STREET
MADISON, WISCONSIN 53703-3398
TELEPHONE (608) 255-7277
FACSIMILE (608) 255-6006
EMAIL: wheeler@wheelerlaw.com

OF COUNSEL

CHARLES S. VAN SICKLE
NORMAN C. ANDERSON
JANET L. KELLY

FLOYD E. WHEELER
(1905 - 1995)

NILES BERMAN
JEFFREY L. LANDSMAN
THOMAS J. ZAREMBA*
STUART G. MONDSCHNEIN†
COURT COMMISSIONER
WILLIAM PRAY O'CONNOR
DENIS R. VOGEL**
RHEA A. MYERS
MARY BETH PERANTEAU
JAY DONALD JERDE**
JUSTIN W. CHASCO

* ALSO ADMITTED IN MICHIGAN
** ALSO ADMITTED IN MINNESOTA
† Certified Civil Trial Advocate, National Board of Trial Advocacy

May 17, 2010

Mr. Michael Lutz
Section Chief, General Counsel
WI Department of Natural Resources
P.O. Box 7921
Madison, WI 53707-7921
Re: Okauchee Lake Management District

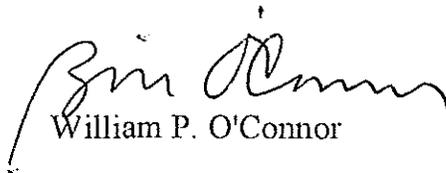
Dear Mike:

I am enclosing a copy of the Agreement between the Okauchee Lake Management District and the Department regarding the Department's boat launch in the Town of Oconomowoc on Lake Okauchee. For the record, the Agreement was recorded in the office of the Waukesha County Register of Deeds on April 20, 2010, as Document Number 3739267.

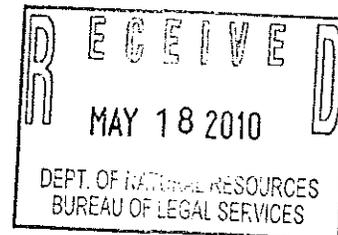
On behalf of the Lake District, I hope you will convey to the Secretary our appreciation for the Department's cooperation in resolving this matter.

Very truly yours,

WHEELER, VAN SICKLE & ANDERSON, S.C.


William P. O'Connor

WPO/lac
Enclosure
cc: Tom Godar



	Agreement	<p>3739267</p> <p>REGISTER OF DEEDS WAUKESHA COUNTY, WI RECORDED ON</p> <p>April 20, 2010 09:54 AM James R Behrend Register of Deeds</p> <p>4 PGS TOTAL FEE: \$17.00 TRANS FEE: \$0.00 Book Page -</p> 
--	------------------	---

THIS AGREEMENT is effective this 5th day of February, 2010, by and between the Wisconsin Department of Natural Resources (the "Department") and the Okauchee Lake Management District (the "District").

WHEREAS, the District is a Wisconsin municipal corporation organized pursuant to Chapter 33 of the Wisconsin Statutes with powers necessary to carry out a program of lake protection and rehabilitation of Okauchee Lake, located in the Towns of Merton and Oconomowoc, Waukesha County, Wisconsin; and

WHEREAS, the Department is an agency of the sovereign State of Wisconsin with primary jurisdiction over the waters of the State, including Okauchee Lake; and

WHEREAS, the Department is the owner of a parcel of land described as

Lot No. One(1) of the Plat of Maplewood, a subdivision of part of the South East Quarter of Section Thirty-five (35), Township Eight (8) North, of Range Seventeen (17) East, Town of Oconomowoc, Waukesha County, Wisconsin.

(the "Property"); and

WHEREAS, the Department currently operates and proposes to expand a public boat launch on the Property, including public parking, fishing and related facilities that provide access to Okauchee Lake; and

WHEREAS, other lands in said Section 35 owned by Hans Weissgerber and known as The Golden Mast, include a boat launch facility and associated parking for vehicles and trailers that provide additional opportunities for boaters and fishers seeking access to the waters of Okauchee Lake; and

WHEREAS, the Natural Resources Board has authorized the Department to acquire and develop the parcel adjacent to the parcel it already owns in said Section 35 in order to expand its existing boat launch; and

WHEREAS, the District and the Department wish to provide for a coordination of public and private facilities, including an expanded public boat launch facility on the Property in order to provide reasonable public access to Okauchee Lake and to prevent the development of public access facilities that exceed maximum access all in accordance with Chapter NR 1.91 of the Wisconsin Administrative Code; and

WHEREAS, the District and the Department desire and intend to provide for the development of the Property in a manner that accounts for the availability of other public and private boat launch facilities serving Okauchee Lake, is consistent with Chapter NR 1.91 and which provides an appropriate opportunity for public participation in the review of a plan for the development and operation of the Property as a public facility and

Name and return address:
Prepared by
William P. O'Connor
Wheeler, Van Sickle & Anderson, S.C.
25 W. Main Street, Suite 801
Madison, WI 53703-3398

Parcel Identification Number (PIN): _____
OCOT 0572001

*Pa
17
4*

an analysis of the impact of the development and use of the facility on the human environment.

NOW THEREFORE, the Department and the Owner agree as follows:

1. The Department will prepare a detailed plan for the development and operation of the Property for use as public boat launch and shore fishing access facility, including the number and location of car and car-trailer parking spaces, shore fishing, lighting, erosion control and other facilities, vegetation to provide habitat and to buffer the site from adjacent residential property, hours of operation, site supervision and maintenance, hereinafter the "Site Plan".
2. The Department will hold a public informational meeting on any proposed site plan. As part of that meeting and development effort, the Department will assess the environmental and natural resources issues associated with the proposed development of the site and prepare a written evaluation of the anticipated environmental impacts of the proposed development, including construction and operation of the site and the effects of increased use of boats on navigation and lake habitat.
3. The Department's public informational meeting will provide the District and members of the public with an opportunity to comment on the proposed Site Plan and Environmental Evaluation. The Department shall deliver a notice stating the time and place of the public informational meeting and copies of the proposed Site Plan and Environmental Evaluation to the District, the Town of Oconomowoc, the Waukesha County Park and Planning Department and media sources in Waukesha County at least 30 days prior to the public informational meeting.
4. The Department has promulgated Chapter NR 1.91, Wis. Admin. Code, which establishes policies for the acquisition and development of public boating access sites. Under §§ NR 1.91(4) and (5), the Department establishes minimum and maximum access standards, which are based on the number of qualified car-trailer parking spaces available at public and private boat launch facilities. Car-trailer parking spaces at public facilities owned by the Department and other public agencies and those provided at private facilities for a reasonable fee, subject to a Private Provider Agreement with the Department under § NR 1.91(7)(c) constitute qualified car-trailer parking spaces and are referred to in this Agreement as "Qualified Spaces." Handicapped accessible sites do not count toward the maximum allowable access standards pursuant to §§ NR 1.91(4)(d)2 and (5)(c).
5. The Property is currently improved with 18 Qualified Spaces, including one such space that is restricted to use by handicapped persons. The DNR intends to construct on the Property, an additional six Qualified Spaces and five additional car-only spaces restricted to use by persons engaged in shore fishing. The Golden Mast Inn presently provides 13 Qualified Spaces, including one such space that is restricted to use by handicapped persons, under a Private Provider Agreement. These provide for a total of 32 qualified spaces, 30 of which count toward the determination of minimum and maximum public access standards under §§ NR 1.91(4) and (5). In addition, the Golden Mast provides at least 45 additional car-trailer parking spaces providing access to Okauchee Lake that are not Qualified Spaces because they are not subject to a Private Provider Agreement, which are referred to in this Agreement as "Non-qualified Spaces."
6. The Department will not construct additional car-trailer parking spaces on the Property or authorize or pursue additional public access facilities (as defined by sec. NR 1.91, Wis. Adm. Code) on the lake, except in accordance with this Agreement.
7. The Department has determined that 35 Qualified Spaces, plus the two handicapped spaces will provide reasonable public access to Okauchee Lake (including those provided by the Department and by Private

Provider Agreement) for so long as the Golden Mast continues to provide at least 10 additional Non-qualified Spaces. The Department may increase the number of public access parking spaces provided at the Property, as follows: if the number of Qualified Spaces provided by the Golden Mast is reduced, the Department may increase the number of Qualified Spaces at the Property by one space for each Qualified Space that is discontinued at the Golden Mast, provided that the total number of such spaces does not exceed 35. Notwithstanding the foregoing, if the number of Non-qualified Spaces at the Golden Mast is reduced below 30, the Department may increase the number of car-trailer parking spaces provided at the Property by 1 space for each Non-qualified Space that is discontinued at the Golden Mast, provided the total number of such parking spaces on Okauchee Lake does not exceed 40.

- 8. This Agreement constitutes an intergovernmental agreement between the Department and the District. Paragraphs 1, 2 and 3, above, are intended to establish a process for public participation in the Department's development of the Property and the District waives, remises, and releases any rights, claim, or cause of action which the District has now, or which the District may have in the future against, and covenants not to sue, the Department and/or its past, present, and future officers, officials, directors, employees, and agents, due to the site plan for the property, the written analysis required under this agreement or the lack thereof. Paragraph 6 of this Agreement constitutes a negative easement in gross over the Property held by the District pursuant to Section 700.40 of the Wisconsin Statutes.
- 9. A qualified officer of the Department shall re-execute this Agreement and record the same in the office of the Waukesha County Register of Deeds.
- 10. The undersigned Secretary of the Department of Natural Resources and the undersigned Chairperson and Secretary of the Okauchee Lake Management District represent that they are authorized to execute this agreement under the laws of the State of Wisconsin.

IN WITNESS WHEREOF, the parties have signed this agreement this 5th day of February, 2010.

OKAUCHEE LAKE MANAGEMENT DISTRICT

BY: Carol A Wilson
Carol Wilson, Chairperson

ATTEST: Tom Godar
Tom Godar, Secretary

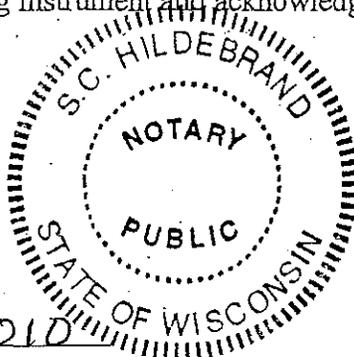
STATE OF WISCONSIN
DEPARTMENT OF NATURAL RESOURCES

BY: Matthew Frank
Matthew Frank, Secretary

STATE OF WISCONSIN)
)
) ss.
COUNTY OF WAUKESHA)

Personally came before me this 12th day of April, 2010, the above-named Carol Wilson and Tom Godar, to me known to be the Chair and Secretary, respectively of the Okauchee Lake Management District, and in said capacity, executed the foregoing instrument and acknowledged the same.

S. C. Hildebrand
S. C. Hildebrand
(print name)



Notary Public, State of Wisconsin
My Commission expires 12/26/2010

STATE OF WISCONSIN)
)
) ss.
COUNTY OF DANE)

Personally came before me this 5th day of February, 2010, the above-named Matthew Frank, Secretary of the Wisconsin Department of Natural Resources, to me known to be the person who executed the foregoing instrument and acknowledged the same in the capacity therein stated and for the purpose therein contained.

Michael A Lutz
Michael A Lutz
(print name)

Notary Public, State of Wisconsin
My Commission expires is permanent

Project No. 10H2S-01
Updated May, 2014

**S.E. Region Boat Launches
Dept. of Natural Resources**

Storm Water Management Report for Okauchee Lake

INTRODUCTION:

The Department of Natural Resources (DNR), along with the Division of Facilities Development (DFD), would like to improve public boat access. For Okauchee Lake, the DNR has purchased an adjacent property to the existing launch site and would like to improve the current site by providing additional parking and better vehicular traffic patterns. This boat access is quite popular and would benefit from the improved parking and traffic routing. A substantial amount of new pavement is proposed for this site and due to this; water quality and quantity are being addressed with the improvements. A bio-filtration basin is proposed that would collect the storm water runoff from the majority of the site. This basin would contain two cells that would be connected by a culvert pipe that would allow water to flow freely between the cells. An existing shelter exists on the site and a strip of land between the cells would allow boat launch users to access the shelter without walking through the rain garden. A smaller rain garden is also proposed that would help manage drainage from the driveway primarily. These storm water facilities will help provide water quality, peak flow control and allow seepage into the ground prior to entering the lake.

EXISTING CONDITIONS:

For Okauchee Lake, the existing conditions consist of one drainage area/watershed that encompasses the general boat launch and improvements area, as well as off-site flows that flow directly onto the site improvements area.

Okauchee Lake

The overall existing site conditions and release rates can be summarized as follows:

Area = 2.70 acres

CN = 72.9

$T_c = 25$ min.

1-year $Q_p = 1.01$ cfs

2-year $Q_p = 1.41$ cfs

10-year $Q_p = 3.25$ cfs

100-year $Q_p = 8.07$ cfs

PROPOSED CONDITIONS:

For the Okauchee Lake site, providing storm water quality was the main focus. Since this site should be considered re-development the goal was to **reduce the total suspended solids (TSS) by at least 40%** compared to no controls. Another goal was to reduce the proposed peak flows to pre-development peak flows for the smaller storm events, as these are the most common and affect the overall water quality most. As mentioned previously both a bio-filtration basin (with underdrain) and rain garden are proposed to provide the storm water runoff treatment. For Okauchee Lake, these practices are adjacent to the lake will also provide peak flow control. It

should be noted that larger storm events would overtop the rain garden spillway but the initial flush of runoff would be treated. As modeled using WinSLAMM V10 software, each of the rain gardens proposed would provide 100% TSS removal for the modeled rain events, only the suspended solids from the areas that cannot be routed through a practice should reach the lake.

The developed conditions, release rates and overall TSS removal compared to no controls for proposed conditions are summarized below:

Okauchee Lake

P1 – To Rain Garden No. 1

Area = 2.02 acres

CN = 79.9

T_c = 15.9 min.

1-year Q_p = 1.69 cfs

2-year Q_p = 2.13 cfs

10-year Q_p = 4.05 cfs

100-year Q_p = 8.67 cfs

P2 – To Rain Garden No. 2

Area = 0.29 acres

CN = 73.3

T_c = 12 min.

1-year Q_p = 0.16 cfs

2-year Q_p = 0.22 cfs

10-year Q_p = 0.48 cfs

100-year Q_p = 1.17 cfs

P3 – Undetained to Lake

Area = 0.39 acres

CN = 66.1

T_c = 15 min.

1-year Q_p = 0.06 cfs

2-year Q_p = 0.11 cfs

10-year Q_p = 0.35cfs

100-year Q_p = 1.05 cfs

Bio-Filtration Basin No. 1

This proposed bio-filtration basin would have two cells that would be interconnected by a 12-inch diameter PVC culvert pipe. This pipe would allow water to transfer between the cells. A flat strip of grass between the cells would provide access to the existing shelter located between the basin and the lake. The northerly cell would contain the outlet structures for the bio-filtration basin. A 2-foot diameter riser pipe would be connected to the 4" diameter under drains, as shown on the construction plans. Additionally, an 8-inch outlet pipe (elev. 875.25), located one-foot above the rain garden bottom (elev. 874.25), would allow runoff from larger storm events to drain out of the basin and limit inundation. The one foot of storage below allows smaller events to seep through the basin bottom and slowly dissipate to the under drains. Each cell would also consist of

a 5-foot wide spillway (elev. 876.50) that would prevent water from backing up onto the parking lot. Test pits were done in the area for the proposed basin, the results of the soils analysis concluded that infiltration should not be promoted due to seasonal high groundwater. At the time of soil test observation the groundwater was well below seasonal observations. For the peak outflow shown below, the under drain and outlet structure flows are accounted for since both would be piped to the lake.

1-year $Q_{p(out)} = 0.36$ cfs	Max. water surface elevation: 875.20'
2-year $Q_{p(out)} = 0.42$ cfs	Max. water surface elevation: 875.34'
10-year $Q_{p(out)} = 1.13$ cfs	Max. water surface elevation: 875.82'
100-year $Q_{p(out)} = 3.85$ cfs	Max. water surface elevation: 876.65'

Rain Garden No. 2

This proposed rain garden would be significantly smaller than rain garden No. 1 but treats a much smaller drainage area. This rain garden contains a small outlet pipe that would work manage runoff, along with the expected infiltration, to control the outflow and water surface elevation. Based on the test pit analysis, sand and gravel was found a couple feet below the ground surface. Construction of the rain garden, based on the grading plan, would indicate the bottom of the garden (elev. 874.50) will be within this sand and gravel layer and therefore an infiltration rate of 3.6 inch/hour is expected and used for the model. The seasonal groundwater elevation was well below the proposed basin bottom and should not be a concern. The garden would consist of a 5-foot wide spillway (elev. 875.25).

1-year $Q_{p(out)} = 0.02$ cfs	Max. water surface elevation: 874.78'
2-year $Q_{p(out)} = 0.02$ cfs	Max. water surface elevation: 874.89'
10-year $Q_{p(out)} = 0.10$ cfs	Max. water surface elevation: 875.29'
100-year $Q_{p(out)} = 0.48$ cfs	Max. water surface elevation: 875.36'

Total Proposed vs. Pre-Developed Conditions Peak Flow Comparison

<u>Storm Event</u>	<u>Proposed</u>	<u>Pre-Developed</u>
1-year	$Q_p = 0.43$ cfs	$Q_p = 1.01$ cfs
2-year	$Q_p = 0.49$ cfs	$Q_p = 1.41$ cfs
10-year	$Q_p = 1.33$ cfs	$Q_p = 3.25$ cfs
100-year	$Q_p = 4.44$ cfs	$Q_p = 8.07$ cfs

Water Quality Calculations

	Solids before controls (lbs)	Solids after controls (lbs)	Total % removed
P1 - To Rain Garden No. 1	119.0	0.0	
P2 - To Rain Garden No. 2	18.6	0.0	
P3 - Undetained to Lake	27.5	27.5	
Total:	165.1	27.5	83.4%

Conclusion

Based on the results of the modeling, the goals for the storm water management have been met. For Okauchee Lake significant additional impervious surface is being introduced and due to this, addressing both water quantity and water quality were goals. For water quantity, proposed peak flow will be kept below pre-developed peak flows. For water quality, the goal was to reduce total suspended solids by at least 40%. The proposed storm water management plan meets the goals set for the boat launch improvements.

PASER Evaluation
Okauchee Lake Pubic Access Site Parking Lot
Report dated April 22, 2005

Site Visit

Site Visit on April 19, 2005
Lynette Check, SER Engineer
Pictures taken and attached to this report.

Written Evaluation

Lynette Check
SER Engineer

History of the Parking Lot

The Department in 1976 purchased Okauchee Lake boat access. In 1979 – 80 the boat launch and parking lot were developed on the site.

The parking lot has deteriorated over the years causing some areas to collect standing water and develop potholes. Each year the parking lot is patched with asphalt.

Evaluation

The top coat of the asphalt is gone giving the parking lot a moderate look of ravelling. There is large black cracking throughout the parking lot. The cracks vary in width from 1/2" to 2". There were potholes but they have been patched this year. The lot has many potholes that have been patched over the years. There is several cracking along the seams of the asphalt paving. There are some low spots through out the parking lot where water is collecting. The corners of the parking lot have severe cracking.

A rating of 3 has been given to the Okauchee Lake public access site parking lot. The parking lot should be pulverized, regraded and then resurfaced. The sub-grade looks good but will be checked once the pulverizing is done.

poration, and shall be structured through articles of incorporation and by-laws to direct its mission and activities to the support of the property, group of properties, or other department facilities and programs as approved by the department.

2. The department shall enter into written agreements with each qualifying friends group to ensure statewide consistency and fiscal accountability. The agreements shall, at a minimum, include provisions that require friends groups to:

- a. Provide an annual fiscal and program report to the department, which may not include a list of donors or itemized donations,
- b. Provide meeting notice which is reasonably likely to apprise interested persons,
- c. Permit a fiscal audit by the department upon request,
- d. Maintain non-profit status,
- e. Provide liability insurance indemnifying the department if requested by the department,
- f. Prohibit department employees from serving as officers or directors,
- g. Agree to not represent their employees and volunteers as department employees,
- h. Permit the department to promote the friends group in its publications and announcements,
- i. Distribute assets upon dissolution to another not-for-profit corporation benefiting the same property, another DNR friends group, the Wisconsin natural resources foundation, inc., or the department. The agreements will include a provision for termination upon reasonable notice by either party.

3. The department may assist friends groups by making department facilities and equipment available to them, dependent upon availability, and as specified in written agreements.

4. Permanent department employees may be members of a friends group, but may not serve as directors or officers of the group. Department employees may, incidental to their regular duties, sell items on behalf of the friends group.

History: Cr. Register, July, 1989, No. 403, eff. 8-1-89; CR 07-026: am. (4) (b) 4. Register December 2007 No. 624, eff. 1-1-08.

NR 1.90 Public access policy for waterways. (1) It is the goal of the state of Wisconsin to provide, maintain and improve access to the state's navigable lakes, rivers and streams for the public. Public access facilities shall allow for public rights of navigation, related incidental uses and other uses which are appropriate for the waterway. Waterway uses shall be equally available to all waterway users and include enjoyment of natural scenic beauty and serenity. These public rights and uses may be provided by any combination of publicly and privately owned access facilities which are available to the general public free or for a reasonable fee. The department, alone or in cooperation with local government, shall exercise its management and regulatory responsibilities to achieve this goal and to assure that levels and types of use of navigable waters are consistent with protection of public health, safety and welfare, including protection of natural resources.

(2) The department shall:

- (a) Acquire, develop, maintain and improve navigation access which meets policy objectives;
- (b) Provide public access to lands adjacent to state waters for uses that are not directly related to navigation but which require or are enhanced by proximity to water;
- (c) Work with local units of government, other state and federal agencies and citizens to acquire, develop, maintain and improve public access;
- (d) Work with private access providers to acquire, develop, maintain and improve access available to the public;
- (e) Develop standards for public access acquisition, development, maintenance and improvement that provide recreational opportunities consistent with demand, commensurate with the

capacity of the resource to support recreation and that provide a broad range of recreational experiences;

(f) Provide funding and services that enhance natural resource values of a waterway only if reasonable public boating access has been provided;

(g) Enhance development of non-boating public access throughout the state to accommodate a significant population of disabled, elderly and others who elect not to use watercraft to enjoy and use the state's waterways;

(h) Increase public awareness of water user responsibilities, public safety and measures to protect the natural resource values of our lakes, rivers and streams; and

(i) Work with local units of government and citizens to help reduce use conflicts and trespass problems.

History: Cr. Register, October, 1977, No. 262, eff. 11-1-77; r. and recr. Register, March, 1994, No. 459, eff. 4-1-94.

NR 1.91 Public boating access standards.

(1) **APPLICABILITY.** Sections NR 1.91 to 1.93 shall apply to department decisions related to acquiring, developing, maintaining and improving public boating access sites, providing natural resources enhancement services and to other department decisions relating to protection and use of navigable waters. Sections NR 1.91 to 1.93 do not allow trespass across private lands, change existing trespass law nor change navigation laws. Sections NR 1.91 to 1.93 would not apply to waterways which are not public navigable waters such as most artificial manmade ponds. The rights of riparian owners for singular access to adjacent waterways would also be unaffected.

(2) **DEFINITIONS.** As used in ss. NR 1.90 to 1.93, the following definitions apply:

(a) "Abandon", "abandonment", "discontinuance" means a permanent or long-term closure of an access site whether by resolution, ordinance, signing, placement of a physical barrier or by other means that reduces access.

(b) "Access site" means an area of land providing public boat access or carry-in access, which provides parking for vehicles with or without trailers.

(c) "Carry-in access" means access designed only for non-trailer boat launching.

(d) "Natural resources enhancement services" means funding or activities that increase the recreational or environmental values of a waterway. These services include but are not limited to fish stocking, removal or other fish population management, habitat development, financial assistance for aquatic plant harvesting and lake restoration grants as defined in s. NR 191.03 (5).

(e) "Open water acres" means the water body surface which appears as water predominantly devoid of emergent vegetation on recent aerial photographs representative of the navigation season. This determination shall be made by the department and shall include open water acres on all contiguous waters connected by a channel or river commonly navigated by motorized craft.

(f) "Plan" means an alternative boating access and waterway protection plan developed by the department or a local unit of government pursuant to sub. (6).

(g) "Public access", for purposes of s. NR 1.92, means any site providing motor vehicle access to ice-bound waters, public boating access or carry-in access.

(h) "Public boating access" means any site or combination of sites including private sites meeting the provisions of sub. (7) at which the general public may gain legal access to a body of water by the process of launching a boat.

(i) "Reasonable public boating access" means opportunities for public enjoyment and use of navigable waters which:

1. Allow public rights of navigation and related incidental uses of the water which are equal for all,
2. Comply with the standards for boating access established in this policy,
3. Are available free or at a reasonable fee as determined by standards established in this policy, and

4. Assure that levels and types of waterway use by all users do not interfere with public health, safety and welfare.

(j) "Reduced" means lowering the number of parking units available for public use.

(k) "Resident" means a natural person who permanently resides or owns real property within the unit of government maintaining or operating the access site.

(L) "Resource protection services" include but are not limited to nonpoint pollution control grants, loans for municipal sewage treatment facilities, acquisition grants under the urban green space program, lake planning grants, lake protection grants and funding for municipal boating safety patrols and aids to navigation.

(m) "Season pass" means authorization to use boat access facilities provided by the issuing authority when use of the facilities are available from January 1 to December 31 of each year.

(n) "Services level" means that level of public boating access which meets or exceeds the levels described in sub. (4).

(3) PRIORITIES. When acquiring and developing public boating access sites, the following shall have priority, in no prescribed order of importance:

- (a) Sites on waters without boating access.
- (b) Sites on lakes of over 500 open water acres that fail to meet the services level specified in sub. (4).
- (c) Sites, statewide, on waters having the greatest boating demands.
- (d) Sites which will provide public boating access to rivers and carry-in access to streams failing to meet the services level specified in sub. (4).

(4) MINIMUM PUBLIC BOATING ACCESS TO QUALIFY WATERS FOR RESOURCE ENHANCEMENT SERVICES. (a) The department may only provide natural resource enhancement services for a body of water when it determines that the general public has been provided with reasonable public boating access. The department may not provide natural resource enhancement services on waters where public access has been abandoned or reduced without prior approval by the department.

(b) The department may continue to provide enhancement services to waters that do not meet minimum public boating access development standards where the department determines that existing access facilities are sufficient to meet existing public demand for access.

(c) The department may provide resource protection services for pollution abatement or prevention, natural resources protection, public safety or public boating access if public boating access is not available on a waterway.

(d) A waterway has reasonable public boating access and is eligible for natural resource enhancement services when public boating access meets the following standards:

1. Inland Lakes	Minimum Public Boating Access Development
(Open water acres)	
Less than 50 acres	One carry-in access site for 5 vehicles
50 to 99 acres	One or more access sites which in total provide a combination of 5 vehicle and car-trailer units
100 to 499 acres	One or more access sites which in total provide 1 car-trailer unit per 30 open water acres but no less than 5 units for lakes of 50 to 150 open water acres

500 to 999 acres	One or more access sites which in total provide 1 car-trailer unit per 35 open water acres but no less than 17 units for lakes of 500 to 595 open water acres
1000 to 4,999 acres	One or more access sites which in total provide 1 car-trailer unit per 50 open water acres but no less than 29 units for lakes with 1000 to 1450 open water acres
5,000 or more acres	One or more access sites which in total provide 1 car-trailer unit per 70 open water acres but no less than 100 units for lakes of 5000 to 7000 open water acres

Rivers and Great Lakes

Rivers and Lakes Michigan and Superior and their bays	One access site within 5 miles of each incorporated community bordering the shore
Rivers and streams accessed primarily by carry-in	One access site per 10 miles of stream thread
Exceptions	Determined case-by-case based on a plan.

2. In addition to these standards, additional parking for handicapped individuals meeting the federal and state standards shall also be met. Lakes greater than 50 open water acres in size shall be provided with facilities capable of launching a trailered boat unless exempted by an approved plan.

3. Parking shall be contiguous with the launch site unless the department determines that resource protection, spatial restrictions or other factors require a greater distance. At each site parking for persons with disabilities may be provided at a location different from that for the general public if necessary to comply with federal accessibility standards.

4. Public boating access shall be available free or at a fee meeting the requirements of sub. (11).

5. Public boating access support facilities such as toilets, waste containers, lights, etc., shall be provided where necessary for public safety, or to protect resources or resolve conflicts with affected property owners.

6. Public boating access shall, at a minimum, be open during normal operating hours for outdoor public recreational facilities in the vicinity and year-round unless public safety requires closure. An exception may be made when public boating access is provided through agreement with a private provider. Any boat launched during operating hours may not be denied egress from the water at any time through the public boating access. Any designated parking unit when found unoccupied shall be considered available for use regardless of prior occupancy, except those designated for handicap use.

7. Public boating access shall provide for use which is consistent with protection of navigable water and generally enjoyed by all users.

(5) MAXIMUM PUBLIC BOATING ACCESS. (a) Local units of government or the department may pursue more public boating access to waters than is required in sub. (4). The department may pursue public boating access to achieve the maximums in par. (b) unless local governments or the department adopt and implement a plan.

(b) The department has determined that granting permits for boating access on bodies of water where the maximum access standards are exceeded will materially impair navigation and is detrimental to the public interest. The department may not pursue public boating access development nor may it approve permits or provide financial assistance for public boating access beyond the levels described in this subsection unless greater levels are established in a plan.

Inland Lakes	Maximum Public Boating Access
(Open water acres)	
Less than 50 acres	One carry-in access site for 5 vehicles
50 to 99 acres	One or more access sites which in total provide 5 car-trailer units
100 to 499 acres	One or more access sites which in total provide 1 car-trailer unit per 15 open water acres
500 to 999 acres	One or more access sites which in total provide 1 car-trailer unit per 25 open water acres but no less than 33 units for lakes of 500 to 825 open water acres
1,000 to 4,999 acres	One or more access sites which in total provide 1 car-trailer unit per 30 open water acres but no less than 40 units for lakes of 1,000 to 1,200 open water acres
5,000 or more acres	One or more access sites which in total provide 1 car-trailer unit per 50 open water acres but no less than 167 units for lakes of 5,000 to 8,350 open water acres
Rivers and Great Lakes	
Rivers and Lakes Michigan and Superior and their bays	One access site per 5 miles of flowing water or where the department determines additional facilities would exceed resource capacity of that part of the water body.
Rivers and Streams	
Rivers and streams accessed primarily by carry-in	One carry-in site per 10 miles of flowing water
Exceptions	Determined by a plan

(c) Access site development shall meet the criteria in subs. (4) to (7).

(6) **ALTERNATIVE PUBLIC BOATING ACCESS AND WATERWAY PROTECTION PLANS.** (a) Natural resource enhancement services may be provided for waters that have less public boating access than that in sub. (4) (d), and public boating access may be developed that exceeds levels in sub. (5) (b) only if local governments or the department implement a plan. Plans developed by local governments require written approval by the department prior to adoption.

(b) Plans shall identify and assess the effects of waterway use on natural resources, describe mechanisms to protect public safety and natural resources, and identify public boating access that meets the objectives of s. NR 1.90. Plans may apply to individual waters or groups of similar waters. Plans shall consider environmental as well as social and developmental factors which may include:

1. Environmental sensitivity criteria:
 - a. Lake size and irregularity.
 - b. Lake depth and contour.

- c. Sensitive areas for fish, wildlife and aquatic plants.
- d. Nature and composition of fish, wildlife and presence of threatened or endangered resources.
- e. Lake bottom sediment types.
- f. Natural shoreline features.
- g. Sensitivity to exotic species.
- h. Water quality.
- i. River or stream characteristics.
2. Social and developmental criteria:
 - a. Shoreline beauty.
 - b. Shoreland zoning.
 - c. Land use and land cover.
 - d. Traditional, existing and potential water uses.
 - e. Ability of the municipality to regulate land use and development.
 - f. Ability of the municipality to enforce public safety regulations.
 - g. Water use regulations proposed or in effect.
 - h. Proximity to other waters.
 - i. Proximity to population centers.
 - j. Demand for recreational opportunities.
 - k. Impact on public safety.
- L. Presence of culturally or historically significant features.
- m. Trespass problems associated with increased access on rivers and streams.
3. Appropriate levels and types of public access based on a consideration of the issues in subs. 1. and 2.
4. Ability of the municipality to effectively implement the plan.

(c) The department shall approve proposed plans and implementing ordinances if it determines that the plans and implementing ordinances are consistent with protection of public health, safety and welfare, the objectives of s. NR 1.90 and include an accurate analysis of the issues in par. (b). Department decisions related to plan approval may be appealed under ch. 227, Stats. The department shall withhold enhancement services until an approved plan is fully implemented. Public boating access site development shall comply with any approved plan. The department may not approve grants and permits if the decision would conflict with an approved plan.

(d) The sponsor of an approved plan shall publish a summary of the plan as a class I legal notice.

(e) The department may waive the minimum reasonable access standards or the need for an alternative plan where it finds that this would not serve to protect the public rights and interest in the waterway.

(7) **PRIVATE PROVIDERS.** Privately owned public boating access shall be included in any determination of access availability for purposes of compliance with ss. NR 1.91 to 1.93 and provision of resource enhancement services if:

- (a) It is provided free or for a reasonable fee, as defined in sub. (11).
- (b) The owner furnishes an irrevocable contract with the state, agreeing to provide specified public boating access facilities for not less than 5 years, and
- (c) Facilities meet the public boating access site development standards under sub. (8).

(8) **PUBLIC BOATING ACCESS SITE DEVELOPMENT STANDARDS.** In addition to other state and federal requirements, including but not limited to the uniform federal accessibility standards (UFAS) published by the architectural and transportation barriers compliance board (ATBCB), the Americans with disabilities act (P.L. 101-336) accessibility guidelines (ADAAG) and the state of Wisconsin building codes (chs. SPS 361 to 365), the following standards shall apply to acquisition, development and maintenance of

boating access sites for the purpose of determining compliance with ss. NR 1.90 to 1.93:

(a) Natural shoreline beauty shall be protected by preserving or creating adequate vegetative screening for facilities and parking.

(b) The sum of all public boating access sites on a water body shall accommodate multiple types of use appropriate for the waterway. Individual access sites shall be designed to minimize conflicts between uses at the site and on the water body.

(c) The site and support facilities shall be designed and located so as to avoid damage to critical habitat and other environmentally sensitive areas.

(d) Each site shall be designed to provide barrier-free public boating access for persons with disabilities.

(e) Each site shall be clearly marked at public roadways. Fees and hours of operation shall be clearly posted.

(9) **FINANCIAL ASSISTANCE PROGRAMS.** Providing public boating access is a partnership program between state and local units of government. The department may only provide financial assistance for projects which comply with ss. NR 1.90 to 1.93 and other applicable state and federal requirements. The department shall assist municipalities in applying for state financial assistance for renovation, operation or maintenance expenses if the maximum allowable launch fees do not provide enough revenue to pay for these access site expenses.

(10) **MAINTENANCE AGREEMENTS.** When in the best interests of the state, the department may engage the services of others, by written agreement, with or without compensation, for maintenance of state-owned or funded public boating access sites.

(11) **BOAT LAUNCHING FEES.** The department encourages free boat launching. A reasonable launch fee may be charged under authority of s. 30.77, Stats., for the purpose of operating and maintaining a boat access site owned or operated by municipalities, lake management districts and other access providers meeting the provisions of sub. (7). Charging excessive, unjustified or unreasonable boat launching fees restricts or prohibits public boating access and use of navigable waters in the state. A reasonable launch fee for the purposes of s. 30.77, Stats., is one that does not exceed the maximum allowable amount under the following criteria:

(a) *Base fee.* A base is that fee that is charged a state resident vehicle for entrance to the state parks.

(b) *Public boating access surcharges.* Municipalities, lake management districts and other public boating access providers that maintain any of the following services may add to the base fee not more than the following surcharges for vehicles with trailers. No more than the base fee may be charged for non-motorized or non-trailer boats.

1. Attendant when on duty	.20 X Base
2. On-site toilet facilities	.20 X Base
3. Great Lakes sites	.30 X Base
4. Boats 20 ft. in length or more but less than 26 ft.	.30 X Base
5. Boats 26 ft. or greater in length	.60 X Base

(c) *Daily launch fee.* The total of base fee and all applicable surcharges, rounded to the nearest quarter of a dollar, shall constitute the daily launch fee. A daily launch fee that is paid shall be valid for all boat access facilities provided by the issuing authority for that day. If different fees are charged by the issuing authority for different access sites, the higher fee shall be allowed for use of all the sites.

(d) *Season pass.* If a launch fee is charged, a season pass at a fee not to exceed 10 times the daily launch fee shall be provided for both residents and non-residents. A mechanism to obtain a season pass shall be provided by the public access provider at the launch site.

(e) *Prior approval required.* Each public boating access provider charging a launch fee in excess of the resident state park daily entrance fee shall provide its fee schedule to the department

for approval prior to its adoption. The fee schedule shall be submitted on department forms available from [the] department's central office. Department approval shall be based solely on demonstration that the provider maintains the facilities or services described in par. (b) that justify charges in excess of the resident state park daily entrance fee and that a season pass is available.

Note: A missing word is shown in brackets.

Note: The department's mailing address is: Department of Natural Resources, P.O. Box 7921, Madison, WI 53707.

(f) *Existing approved fee structures.* Reasonable fees under pars. (a) to (e), do not apply to access sites which the department has determined in a written decision to have a reasonable fee prior to the effective date of this rule.

(g) *Differential fee based on residency.* Local units of government, including lake management districts, which maintain and operate public boating access sites, may charge differential fees on the basis of residency within the unit of government maintaining or operating the access. If a fee is charged, the fees for a non-resident may not exceed 150% of the fee charged a resident and nonresident fees may not exceed the maximum allowable amounts except when par. (b) 4. or 5. are applicable.

Note: For example, with a daily resident entrance fee of \$4.00 for state parks, at an access site on an inland lake with an attendant on duty and toilet facilities, a launch fee for an 18 foot boat may be as high as \$5.50 (4 + 0.2 (4) + 0.2 (4), rounded to nearest 0.25) for both residents and non-residents, and for a 26 foot boat as high as \$8.00 (4 + 0.2 (4) + 0.2 (4) + 0.6 (4), rounded to nearest 0.25) for residents and \$12.00 (8 x 1.5, rounded to nearest 0.25) for non-residents.

History: Cr. Register, October, 1977, No. 262, eff. 11-1-77; r. and recr. Register, March, 1994, No. 459, eff. 4-1-94; am. (2) (d), Register, June, 1995, eff. 7-1-95; correction in (6) (a) and (8) (intro.) made under s. 13.93 (2m) (b) 7., Stats., Register, September, 1999, No. 525; correction in (8) made under s. 13.93 (2m) (b) 7., Stats., Register September 2004 No. 585; correction in (8) made under s. 13.92 (4) (b) 7., Stats., Register February 2012 No. 674.

NR 1.92 Abandonment of access. (1) **NOTICE OF INTENT TO ABANDON AN ACCESS.** (a) Any municipality subject to s. 66.1006, Stats., which proposes to abandon or discontinue any highway, street, alley or right-of-way, which provides public access to a navigable waterway, shall provide a copy of the resolution or ordinance and notify the department at least 10 working days prior to acting on a resolution or ordinance to abandon or discontinue. Within 10 working days of enacting an ordinance or resolution subject to approval under s. 66.1006, Stats., the municipality shall submit a copy of the ordinance or resolution to the department. Upon receipt of the ordinance or resolution, the department shall publish a notice of the proposed abandonment pursuant to the procedures in s. 31.06, Stats. If no hearing is requested, the department shall proceed under sub. (2) to grant or deny the petition.

(b) If a hearing is requested, the department shall hold the hearing as a class 1 contested case in the county in which the public access is proposed to be abandoned. The department shall make its decision based on the standards in sub. (2).

(2) **FINDINGS FOR GRANTING.** The department may grant the petition to abandon or discontinue the public access only if:

(a) Any access sites or part thereof proposed to be abandoned or discontinued is replaced prior to granting the petition; or

(b) The department finds that the access proposed to be abandoned does not contribute to the quality or quantity of public access on the body of water.

(3) **APPROVAL CONDITIONS.** The department may order conditions of approval including, but not limited to, a showing of financial capability of the petitioner to provide and maintain an equivalent or superior replacement public access site, and other conditions related to assurance of protection of the interest of the public in the body of water.

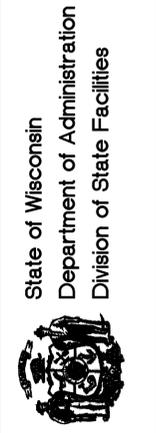
(4) **ENVIRONMENTAL DEGRADATION.** Access sites may also be abandoned where environmental degradation is occurring at the site as a result of existing use, and abandonment of the access will reduce or eliminate the degradation without reducing public interests in access to that body of water.

History: Cr. Register, April, 1975, No. 232, eff. 5-1-75; renum. from NR 1.32, Register, October, 1977, No. 262, eff. 11-1-77; r. and recr. Register, March, 1994,

SOUTHEAST REGION BOAT LAUNCHES OKAUCHEE LAKE

DEPARTMENT OF NATURAL RESOURCES TOWN OF OCONOMOWOC, WAUKESHA COUNTY

PROJECT NUMBER: 10H2S-01



SE REGION BOAT LAUNCHES - OKAUCHEE DEPT. OF NATURAL RESOURCES

Sheet Title: TITLE SHEET

CONTACTS

YAGGY COLBY ASSOC.
CONTACT: BRIAN PEHL, P.E.
PROJECT MANAGER
501 MAPLE AVENUE
DELAFIELD, WI 53018
BRIANP@YAGGY.COM
(262) 646-6855, EXT. 216

DEPARTMENT OF ADMINISTRATION PROJECT MANAGER
CONTACT: JEFF KLAMIK, P.E.
PROJECT MANAGER
101 E. WILSON ST., 7TH FLOOR
MADISON, WI 53707
JEFF.KLAMIK@WISCONSIN.GOV
(608) 267-0423

WISCONSIN DEPARTMENT OF NATURAL RESOURCES
CONTACT: JOHN M. OLSON, P.E.
WDNR - STATE BOAT ACCESS ENGINEER
101 S. WILSON STREET
MADISON, WI 53707-7921
(608) 267-7484

SHEET INDEX

T01	TITLE SHEET
CE01	EROSION CONTROL DETAILS
CD01-CD04	GENERAL DETAILS

OKAUCHEE LAKE

C001	REMOVAL PLAN
C002	GRADING & EROSION CONTROL PLAN
C003	PAVEMENT MARKING PLAN
C004	LANDSCAPE PLAN



Toll Free (800) 242-8511
Milwaukee Area (414) 259-1181
Hearing Impaired TDD (800) 542-2289
www.diggershotline.com

THE LOCATION AND SIZE OF ALL UNDERGROUND STRUCTURES SHOWN ON THESE PLANS HAVE BEEN LOCATED TO A REASONABLE DEGREE OF ACCURACY, BUT THE ENGINEER DOES NOT GUARANTEE THEIR EXACT LOCATION NOR THE LOCATION OF OTHERS NOT SHOWN.

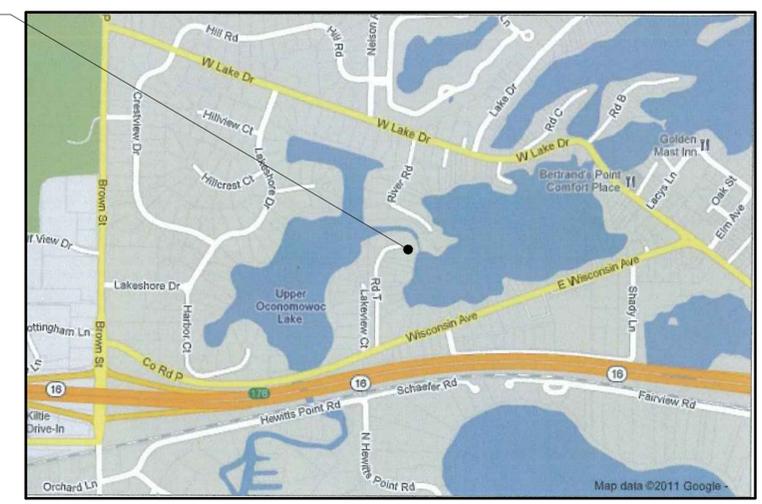
DIVISION PROJECT NO. 10H2S-01
STATE OF WISCONSIN
DEPARTMENT OF ADMINISTRATION
DIVISION OF FACILITIES DEVELOPMENT



WAUKESHA COUNTY, WISCONSIN

LOCATION MAP
N.T.S.

PROJECT LOCATION

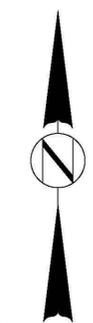
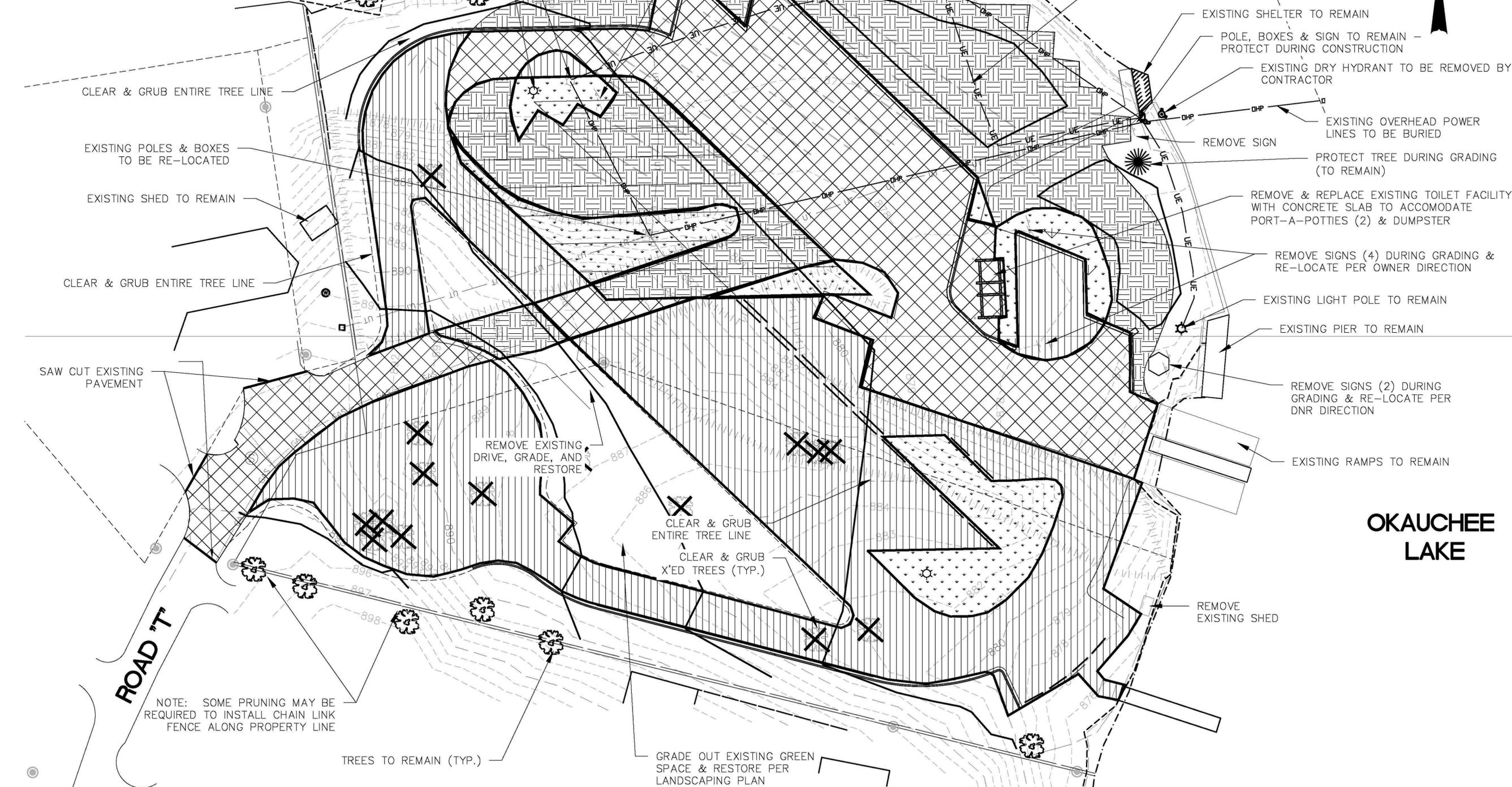


LOCATION MAP
N.T.S.

Revisions:		
No.	Date:	Description:
1	11/16/2011	DSF REVIEW
2	4/9/2014	DNR REVIEW

Graphic Scale	NOT TO SCALE
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	T01

-  EXISTING PAVEMENT TO BE MILLED OR PULVERIZED, PREPARED FOR RE-PAVING USING THE PROPER PAVEMENT AND SUBBASE CROSS-SECTION
-  REMOVE EXISTING PAVEMENT AND BASE, FILL WITH NATIVE MATERIAL, GRADE AND RESTORE PER PLAN
-  EXISTING GREENSPACE TO REMAIN WITHIN PARKING AREA
-  EXISTING GREENSPACE TO BE CONVERTED TO PAVEMENT



OKAUCHEE LAKE

YAGGY COLBY ASSOCIATES
 ENGINEERS
 LANDSCAPE ARCHITECTS
 SURVEYORS PLANNERS
 501 MAPLE AVENUE
 DELAFIELD, WISCONSIN 53018
 262-646-6855
 FAX 262-646-6864
 EMAIL INFO@YAGGY.COM

State of Wisconsin
 Department of Administration
 Division of State Facilities



OKAUCHEE LAKE
 WAUKESHA COUNTY, WISCONSIN

SE REGION BOAT LAUNCHES - OKAUCHEE DEPT. OF NATURAL RESOURCES

Sheet Title:
REMOVAL PLAN

Revisions:

No.	Date	Description
1	11/16/2011	DSF REVIEW COMMENTS
2	4/9/2014	DNR REVIEW

Graphic Scale	
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CO01

NOTE: SOME PRUNING MAY BE REQUIRED TO INSTALL CHAIN LINK FENCE ALONG PROPERTY LINE

NOTE: BOULDERS LOCATED ON NEWLY DEVELOPED PROPERTY SHALL BE MOVED AND RE-USED TO HELP DELINEATE DRIVE BY CONTRACTOR. COORDINATE WITH OWNER FOR LOCATION.

NOTE: ALL EXISTING OVERHEAD POWER LINES TO BE BURIED AS PART OF THIS PROJECT.





YAGGY COLBY ASSOCIATES
 ENGINEERS ARCHITECTS
 LANDSCAPE SURVEYORS PLANNERS
 501 MAPLE AVENUE
 DELAWARE, WISCONSIN 53018
 262-548-6855
 FAX 262-548-6854
 EMAIL INFO@YAGGY.COM

State of Wisconsin
 Department of Administration
 Division of State Facilities



OKAUCHEE LAKE
 WAUKESHA COUNTY, WISCONSIN

SE REGION BOAT LAUNCHES - OKAUCHEE DEPT. OF NATURAL RESOURCES

Sheet Title:
GRADING & EROSION CONTROL PLAN

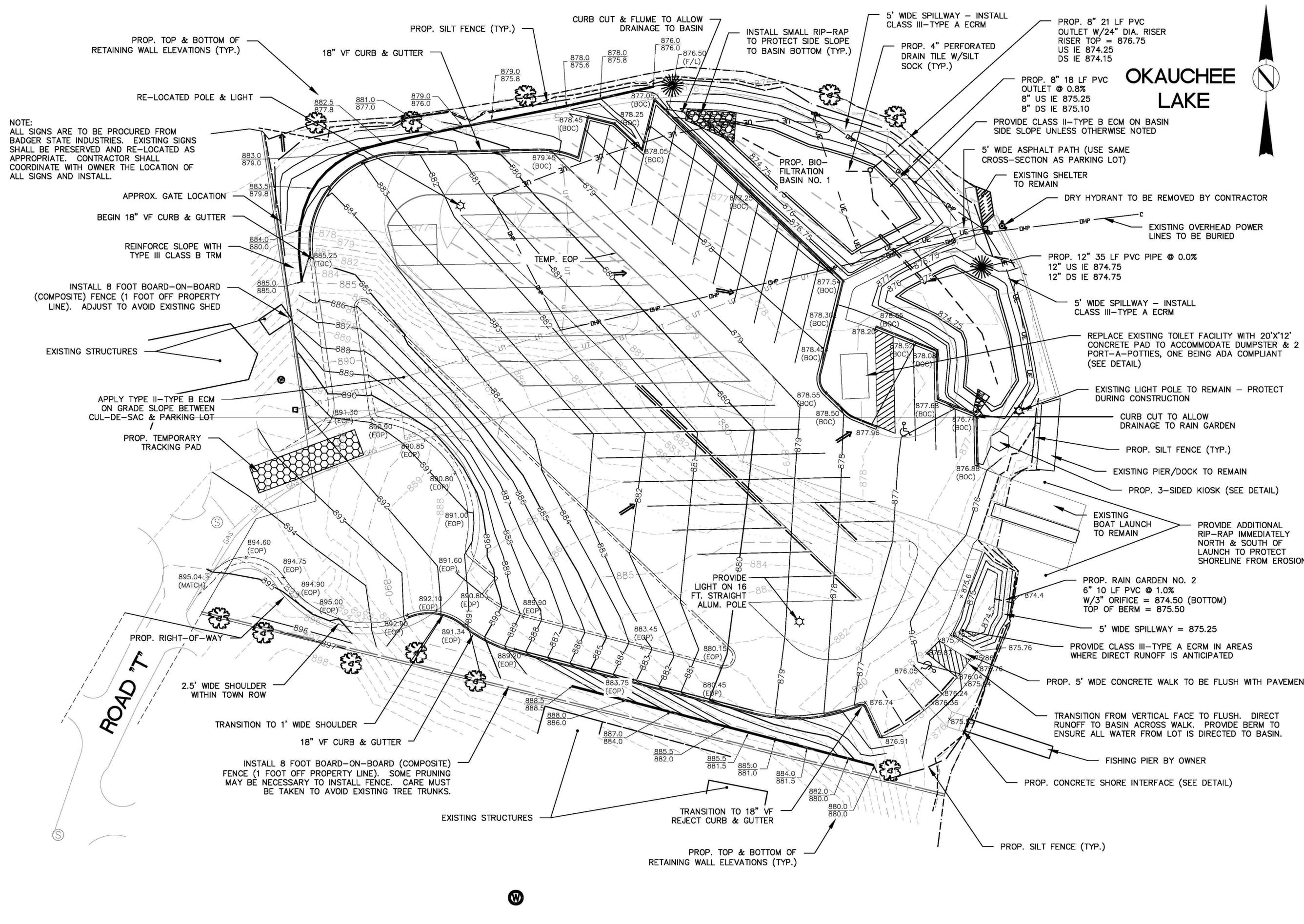
Revisions:

No.	Date:	Description:
1	11/16/2011	DSF REVIEW
2	4/9/2014	DNR REVIEW

Graphic Scale

0'	10'	20'	30'
----	-----	-----	-----

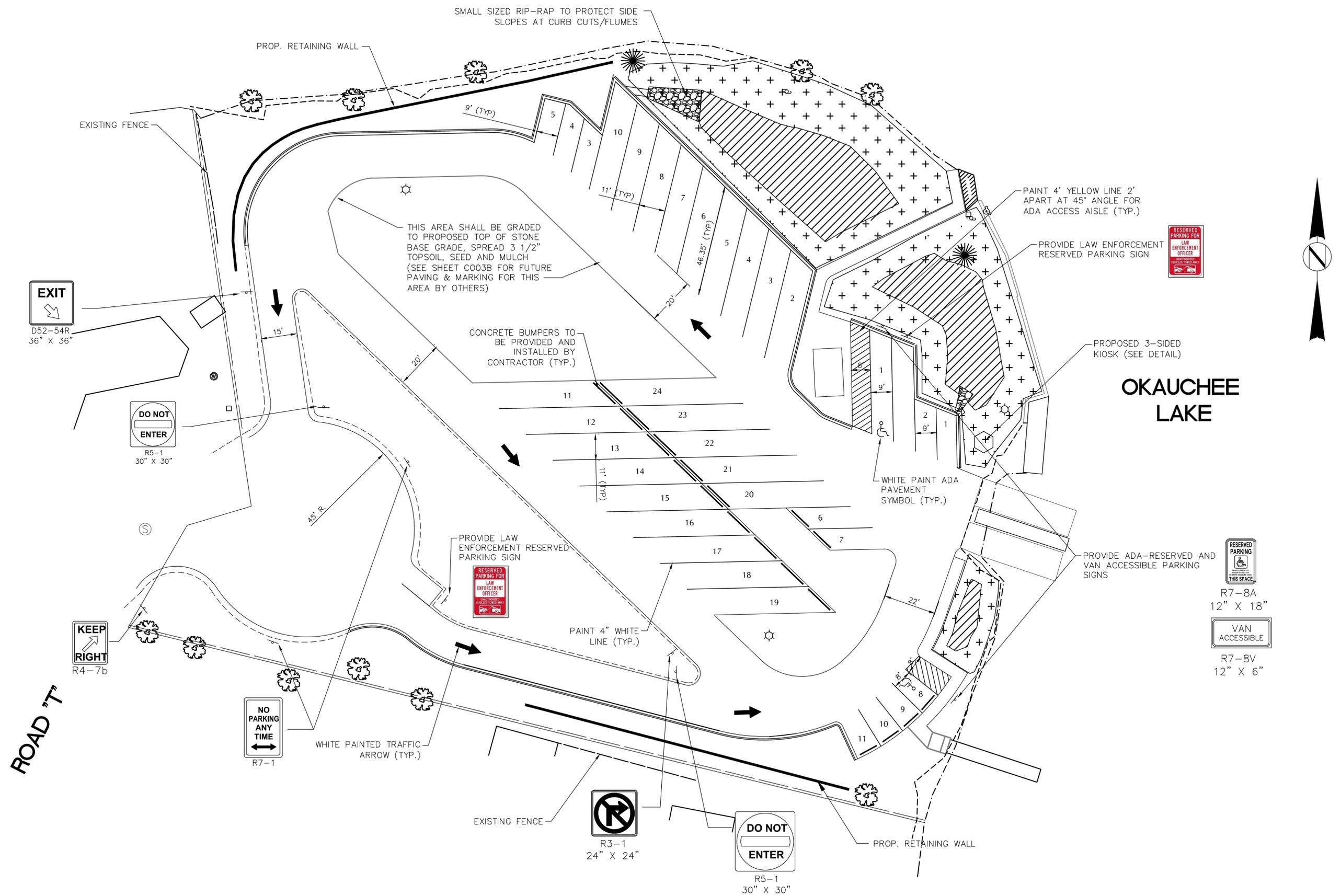
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CO02



Revisions:

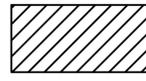
No.	Date:	Description:
1	11/16/2011	DSF REVIEW
2	4/9/2014	DNR REVIEW

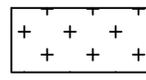
Graphic Scale	
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CO03



NOTE: TYPICAL SIGN MOUNTING HEIGHT = 5 FT FROM BOTTOM OF SIGN

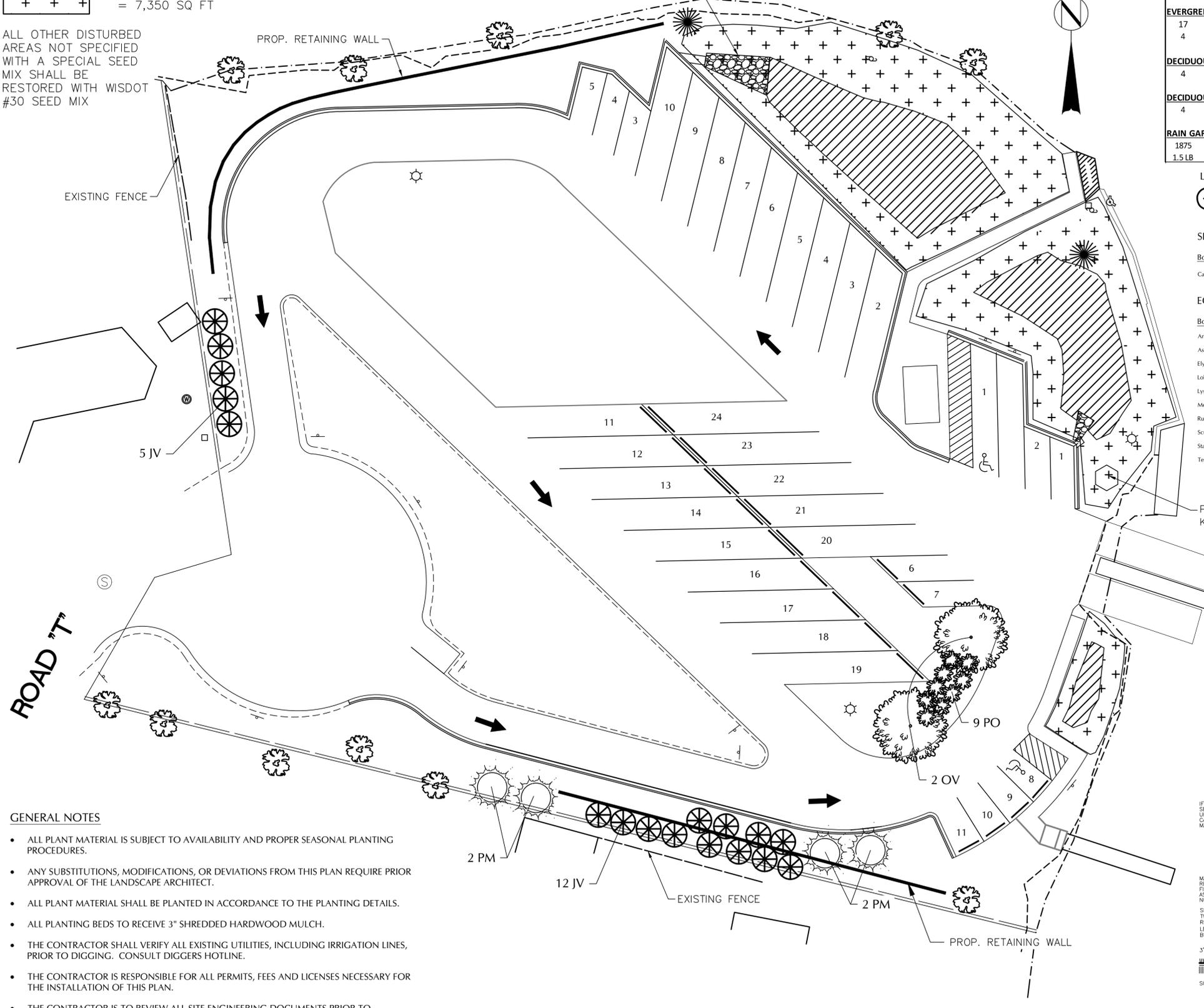


 1875 RAIN WATER RENEWAL PLUGS FROM AGRECOL OR APPROVED EQUAL = 4,220 SQ FT

 1.5 LBS MESIC SEED MIX FROM AGRECOL OR APPROVED EQUAL = 7,350 SQ FT

ALL OTHER DISTURBED AREAS NOT SPECIFIED WITH A SPECIAL SEED MIX SHALL BE RESTORED WITH WISDOT #30 SEED MIX

SMALL SIZED RIP-RAP TO PROTECT SIDE SLOPES AT CURB CUTS/FLUMES

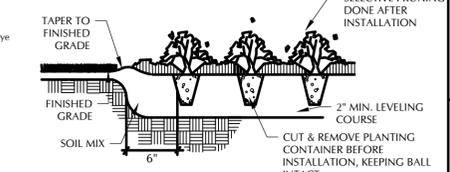
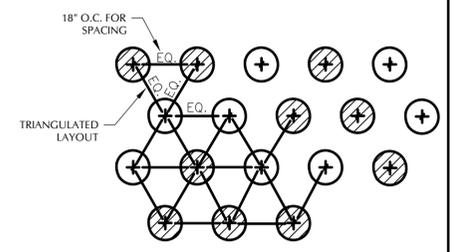


PLANT LIST				
QUANTITIES LISTED HEREIN ARE FOR REVIEW PURPOSES ONLY. PLANT QUANTITIES ILLUSTRATED ON PLANS SHALL BE VERIFIED BY BIDDING CONTRACTOR.				
QTY.	SYM	BOTANICAL NAME	COMMON NAME	SIZE / ROOT COMMENTS
EVERGREEN TREES				
17	JV	JUNIPERUS VIRGINIUM FAIRVIEW	FAIRVIEW JUNIPER	10' HT. BB
4	PM	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	14" HT. BB 18" O.C.
DECIDUOUS TREES				
4	OV	OSTRYA VIRGINIANA	IRONWOOD	3" BB SINGLE TRUNK
DECIDUOUS SHRUBS				
4	PO	PHYSOCARPOS OPULIFOLIUS	DART'S GOLD NINEBARK	36" BB
RAIN GARDEN PLANTINGS				
1875		RAIN WATER RENWAL PLUGS	PLUGS	18" O.C.
1.5 LB		MESIC SEED MIX		

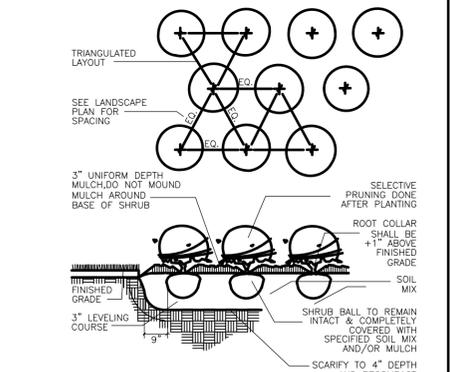
LEGEND
 GRASSES/SEDGES
 WILDFLOWERS

SEDGES:
 Botanical Name: Carex vulpimidea
 Common Name: Fox Sedge

EQUAL AMOUNTS OF WILDFLOWERS:
 Botanical Name: Arisaema dracontium, Aster lateriflorus, Elymus virginicus or E. villosus, Lobelia cardinalis, Lysimachia ciliata, Menispermum canadense, Rudbeckia laciniata, Scutellaria lateriflora, Stachys tenuifolia, Teucrium canadense
 Common Name: Green Dragon, Calico Aster, Virginia Wildrye or Silky Wildrye, Cardinal Flower, Fringed Loosestrife, Common Moonseed, White Golden Glow, Blue Skullcap, Smooth Hedgenettle, Canada Gemander



PLUG PLANTING DETAIL
NTS

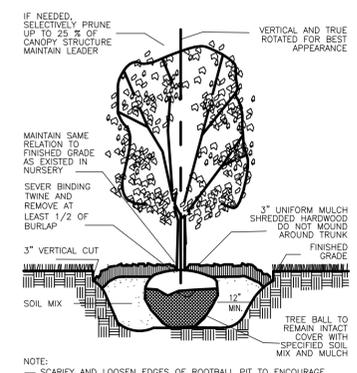


SHRUB PLANTING DETAIL
NTS

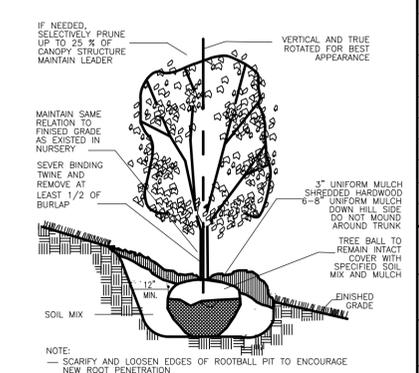
PROPOSED 3-SIDED KIOSK (SEE DETAIL)

OKAUCHEE LAKE

- GENERAL NOTES**
- ALL PLANT MATERIAL IS SUBJECT TO AVAILABILITY AND PROPER SEASONAL PLANTING PROCEDURES.
 - ANY SUBSTITUTIONS, MODIFICATIONS, OR DEVIATIONS FROM THIS PLAN REQUIRE PRIOR APPROVAL OF THE LANDSCAPE ARCHITECT.
 - ALL PLANT MATERIAL SHALL BE PLANTED IN ACCORDANCE TO THE PLANTING DETAILS.
 - ALL PLANTING BEDS TO RECEIVE 3" SHREDDED HARDWOOD MULCH.
 - THE CONTRACTOR SHALL VERIFY ALL EXISTING UTILITIES, INCLUDING IRRIGATION LINES, PRIOR TO DIGGING. CONSULT DIGGERS HOTLINE.
 - THE CONTRACTOR IS RESPONSIBLE FOR ALL PERMITS, FEES AND LICENSES NECESSARY FOR THE INSTALLATION OF THIS PLAN.
 - THE CONTRACTOR IS TO REVIEW ALL SITE ENGINEERING DOCUMENTS PRIOR TO INSTALLATION. ANY CONFLICTS MUST BE REPORTED TO THE LANDSCAPE ARCHITECT. THESE LANDSCAPE DRAWINGS ARE FOR THE INSTALLATION OF PLANT MATERIALS ONLY UNLESS OTHERWISE STATED.
 - STAKE AND LAYOUT ALL PLANT LOCATIONS FOR APPROVAL OF LANDSCAPE ARCHITECT OR OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION.
 - ALL SEEDED AREAS OUTSIDE OF BEDS TO BE INSTALLED BY OTHERS



NOTE:
 - SCARIFY AND LOOSEN EDGES OF ROOTBALL PIT TO ENCOURAGE NEW ROOT PENETRATION
 - MULCH BEDS SHALL BE CIRCULAR UNLESS OTHERWISE NOTED, ONE FOOT PER CALIPER INCH, MINIMUM FIVE FOOT DIAMETER.
 - FOR EVERGREEN TREES MULCH TO ONE FOOT BEYOND DRIPLINE.
 - REMOVE ALL PROTECTIVE WRAPPING AFTER INSTALLATION.



NOTE:
 - SCARIFY AND LOOSEN EDGES OF ROOTBALL PIT TO ENCOURAGE NEW ROOT PENETRATION
 - MULCH BEDS SHALL BE CIRCULAR UNLESS OTHERWISE NOTED, ONE FOOT PER CALIPER INCH, MINIMUM FIVE FOOT DIAMETER.
 - FOR EVERGREEN TREES MULCH TO ONE FOOT BEYOND DRIPLINE.
 - REMOVE ALL PROTECTIVE WRAPPING AFTER INSTALLATION.

TREE PLANTING DETAIL
NTS

YAGGY COLBY ASSOCIATES
 ENGINEERS
 LANDSCAPE ARCHITECTS
 SURVEYORS PLANNERS
 501 MAPLE AVENUE
 DELAFIELD, WISCONSIN 53018
 262-546-8855
 FAX 262-546-8864
 EMAIL INFO@YAGGY.COM

State of Wisconsin
 Department of Administration
 Division of State Facilities

 OKAUCHEE LAKE
 WAUKESHA COUNTY, WISCONSIN

SE REGION BOAT LAUNCHES - OKAUCHEE DEPT. OF NATURAL RESOURCES
 Sheet Title: LANDSCAPE PLAN

Revisions:

No.	Date:	Description:
1	11/16/2011	DSF REVIEW
2	4/9/2014	DNR REVIEW

Graphic Scale
 0' 5' 10' 20' 30'

DSF Number
10H2S-01

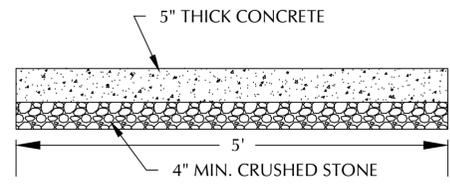
Set Type
BD

Date Issued
01/13/2012

Sheet Number
CO04

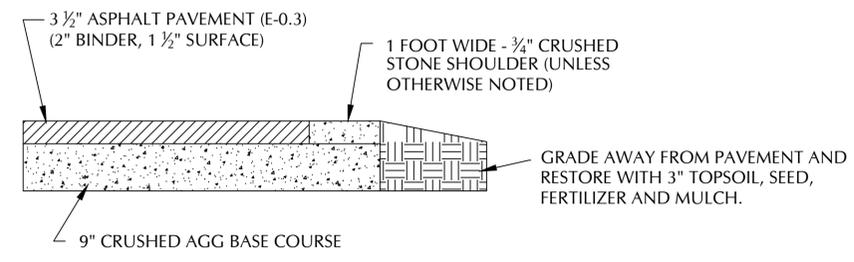
Revisions:		
No.	Date:	Description:
1	11/16/2011	DSF REVIEW COMMENTS
2	4/9/2014	DNR REVIEW

Graphic Scale	 NOT TO SCALE
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CD01



CONCRETE SIDEWALK

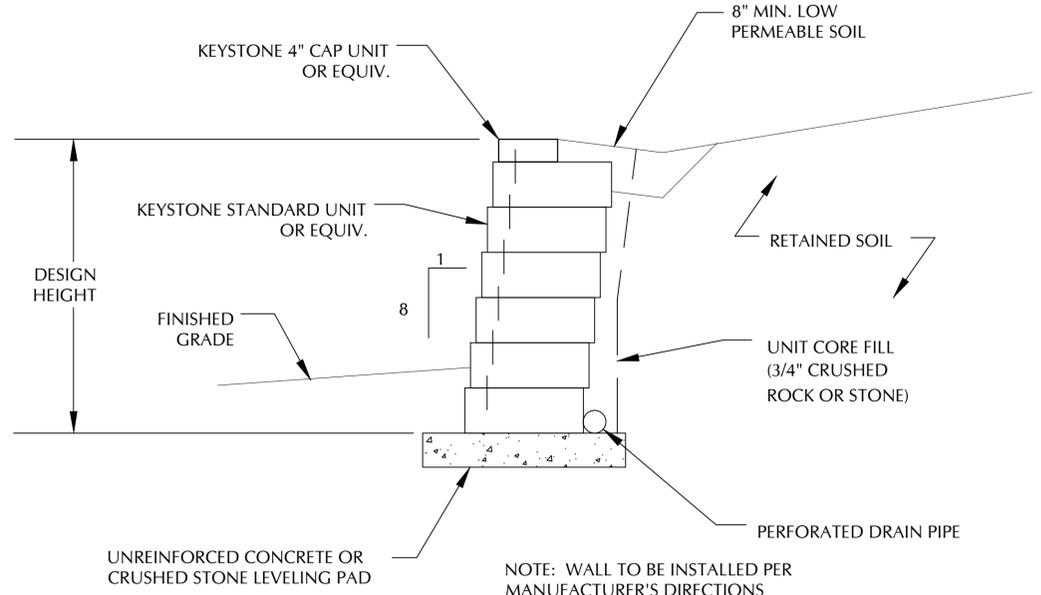
NOT TO SCALE



TYPICAL ASPHALTIC PAVEMENT SECTION

NOT TO SCALE

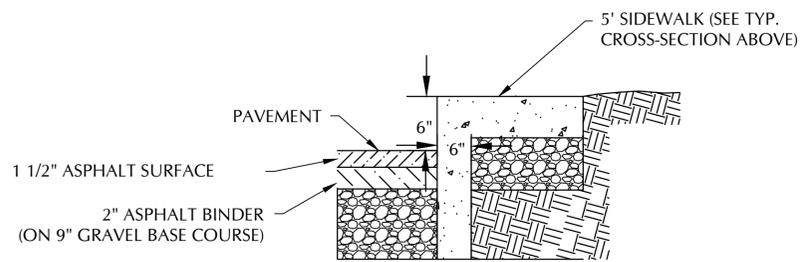
NOTE:
COMPLETE REMOVAL OF EXISTING PAVEMENT AND BASE. EXCAVATE TO NEW SUBGRADE REQUIRED TO ACHIEVE DESIGN PAVEMENT SECTION. EXISTING MATERIALS MAY BE RE-USED IF CONDITION OF MATERIAL IS ACCEPTABLE.



TYPICAL GRAVITY WALL SECTION

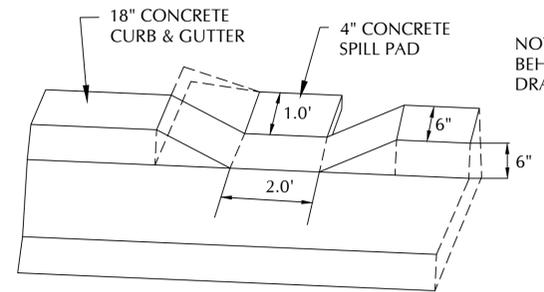
NOT TO SCALE

NOTE: WALL TO BE INSTALLED PER MANUFACTURER'S DIRECTIONS AND SPECIFICATIONS



6" VERTICAL BARRIER CURB & PAVEMENT CROSS-SECTION

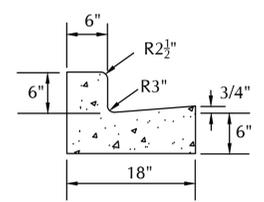
NOT TO SCALE



CURB CUT DETAIL

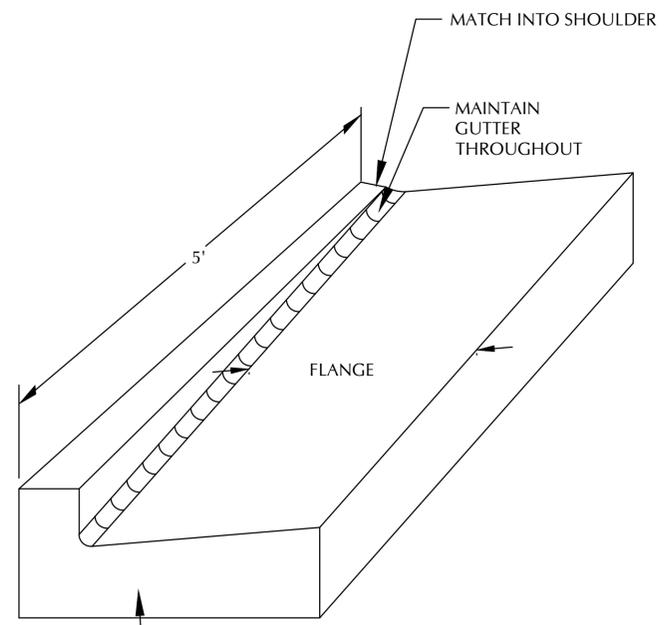
NOT TO SCALE

NOTE: GRADE AREA BEHIND CURB CUT TO DRAIN TO SWM FACILITY



18" CURB & GUTTER

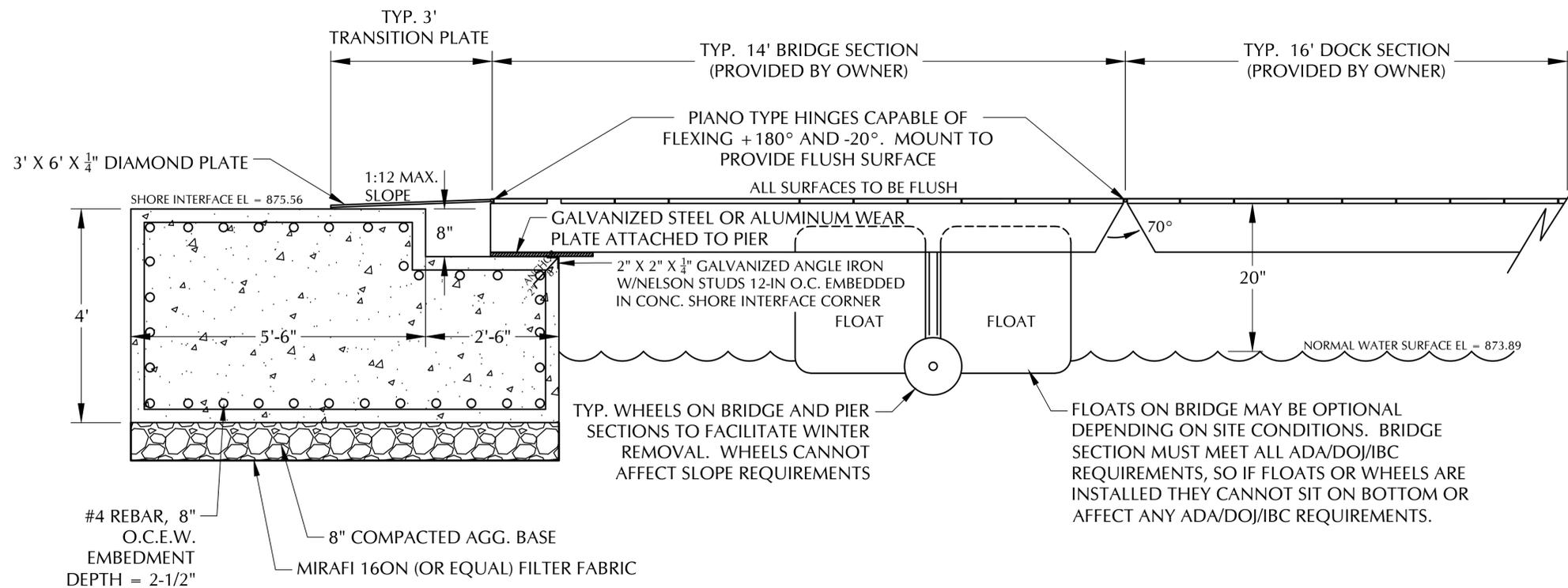
NOT TO SCALE



CURB TAPER

NOT TO SCALE

VERTICAL-FACE CONCRETE CURB (SEE DETAIL)



SHORE INTERFACE DETAIL

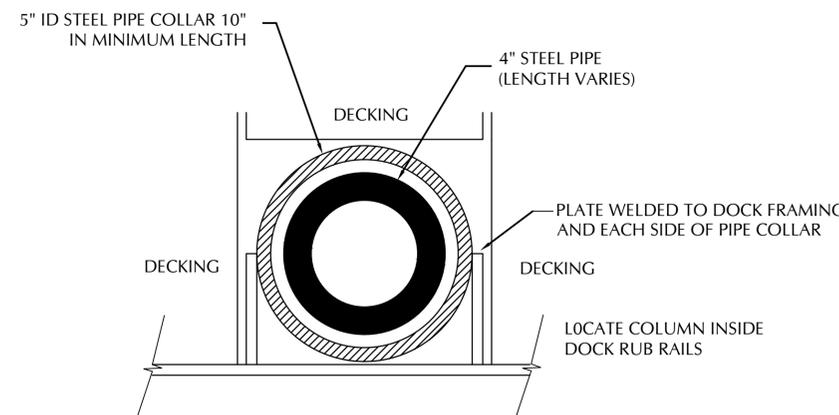
SIDE ELEVATION - NO SCALE

NOTES:

- CONCRETE SHORE INTERFACE TO BE 7' WIDE
- BROOM FINISH SHORE INTERFACE SURFACE PERPENDICULAR TO RAMP
- PROVIDE 1" BEVEL ALONG TWO SIDE EDGES
- PLACE 1/2" EXPANSION JOINT FILLER BETWEEN SHORE INTERFACE AND CONCRETE LAUNCH LANE
- REBAR SHOWN ON DRAWING ARE SCHEMATIC ONLY AND MAY NOT REPRESENT THE ACTUAL NUMBER OF REBAR NEEDED IN CONSTRUCTION. ALL REBAR TO BE #4, 8" O.C.E.W., ALL FACES
- BRIDGE AND DOCK SECTIONS TO BE PROVIDED BY OWNER

SILT FENCE:

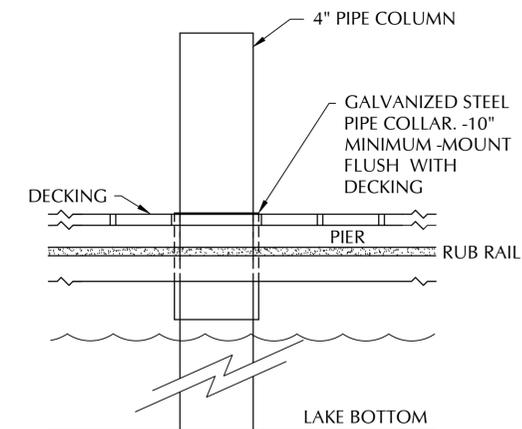
- CONSTRUCT SILT FENCE ON LOW SIDE OF ANY STOCKPILES OR AS INDICATED ON PLANS TO PREVENT SEDIMENT FROM ENTERING SURFACE WATERS.
- LOCATE POSTS DOWN SLOPE OF FABRIC TO HELP SUPPORT FENCING.
- BURY TOE OF FENCING APPROX. 4" DEEP TO PREVENT UNDERCUTTING.
- WHEN JOINTS ARE NECESSARY, SECURELY FASTEN FABRIC AT A SUPPORT POST W/ OVERLAP TO NEXT POST.
- FILTER FABRIC TO BE NYLON, POLYESTER, PROPYLENE, OR ETHYLENE YARN WITH EXTRA STRENGTH - 50 LB/LIN. IN. (MIN.) AND A FLOW RATE OF AT LEAST 0.3 GAL/SQ. FT./MINUTE. FABRIC SHALL CONTAIN ULTRAVIOLET RAY INHIBITORS.
- POSTS TO BE 2x2 WOODEN POSTS DRIVEN AT LEAST 8" DEEP AND SPACED AT MAX OF 3' - FILTER FABRIC SHALL BE STAPLED USING AT LEAST 0.5 INCH STAPLES TO THE UP SLOPE SIDE OF POSTS.
- CONTRACTOR TO REMOVE SILT FENCE ONCE SEEDING SHOWS A GOOD CATCH AND AT PROJECT COMPLETION.



HOT DIP GALVANIZATION REQUIRED AFTER FABRICATION, ALL COMPONENTS.

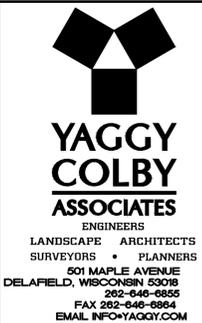
TYP. PIER PIPE ANCHORAGE

PLAN VIEW NOT TO SCALE



TYP. PIER PIPE ANCHORAGE

SIDE VIEW NOT TO SCALE



State of Wisconsin
Department of Administration
Division of State Facilities

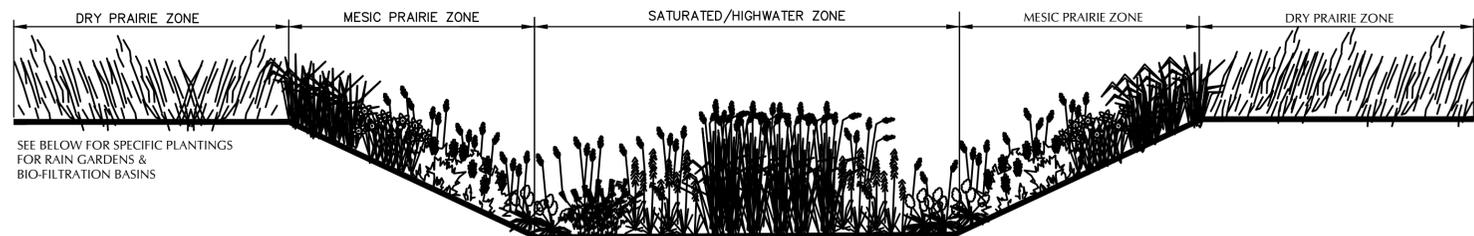


SE REGION BOAT LAUNCHES -
OKAUCHEE
DEPT. OF NATURAL RESOURCES

Sheet Title:
GENERAL DETAILS 2

Revisions:		
No.	Date:	Description:
1	11/16/2011	DSF REVIEW COMMENTS
2	4/9/2014	DNR REVIEW

Graphic Scale	NOT TO SCALE
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CD02

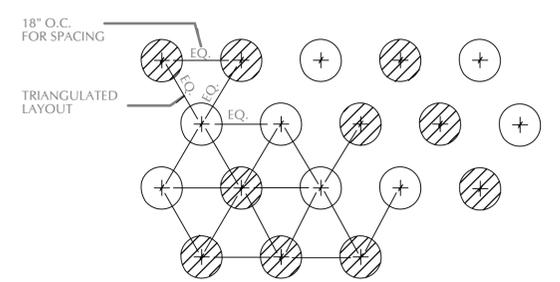


SEE BELOW FOR SPECIFIC PLANTINGS FOR RAIN GARDENS & BIO-FILTRATION BASINS

RAIN GARDENS & BIO-FILTRATION BASINS TO BE EXCAVATED BY EXTRACTION METHOD ONLY. KEEP SCRAPPERS OFF THESE AREAS.

TYPICAL RAINGARDEN & BIO-FILTRATION BASIN PLANTINGS

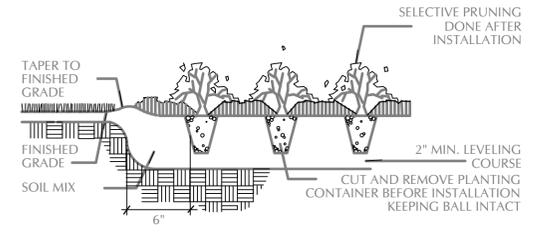
N.T.S.



LEGEND
 (+) GRASSES/SEDGES
 (//) WILDFLOWERS

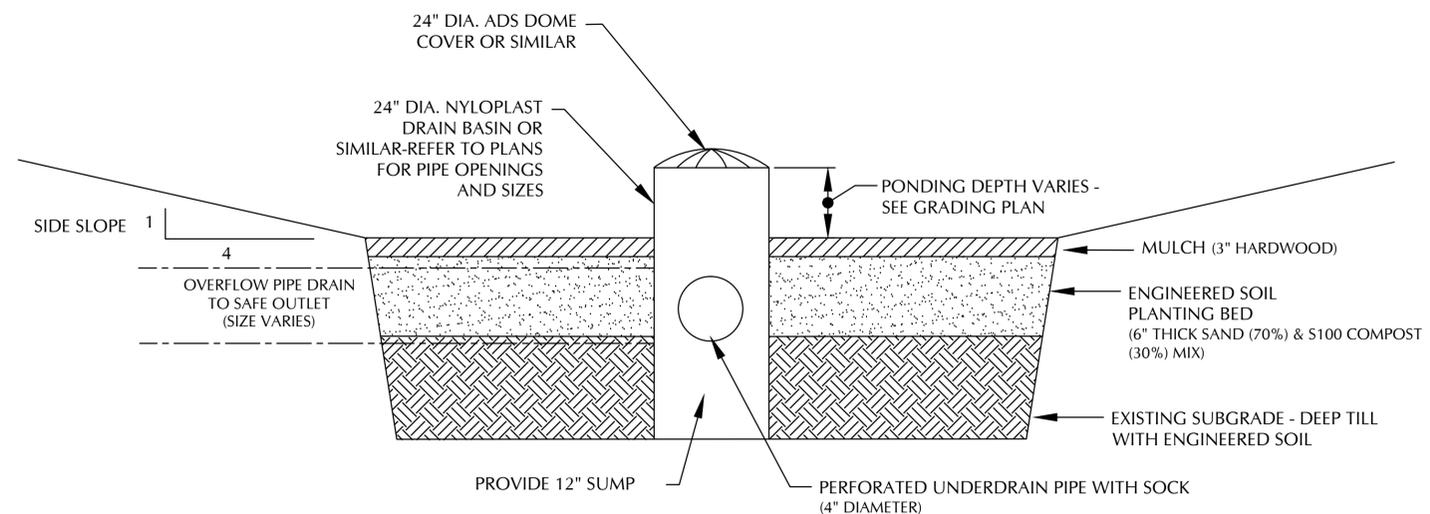
SEDGES:
 Botanical Name: *Carex valpinidea*
 Common Name: Fox Sedge

EQUAL AMOUNTS OF WILDFLOWERS:
 Botanical Name: *Asclepias incarnata* (Red Milkweed)
Aster Novae-angliae (New England Aster)
Eupatorium perfoliatum (Boneset)
Liatris pycnostachia (Prairie Blazing Star)
Ratibida pinnata (Yellow Coneflower)
Rudbeckia submentosa (Sweet Blackeyed Susan)
Solidago rigida (Stiff Goldenrod)
Zizia aurea (Golden Alexander)



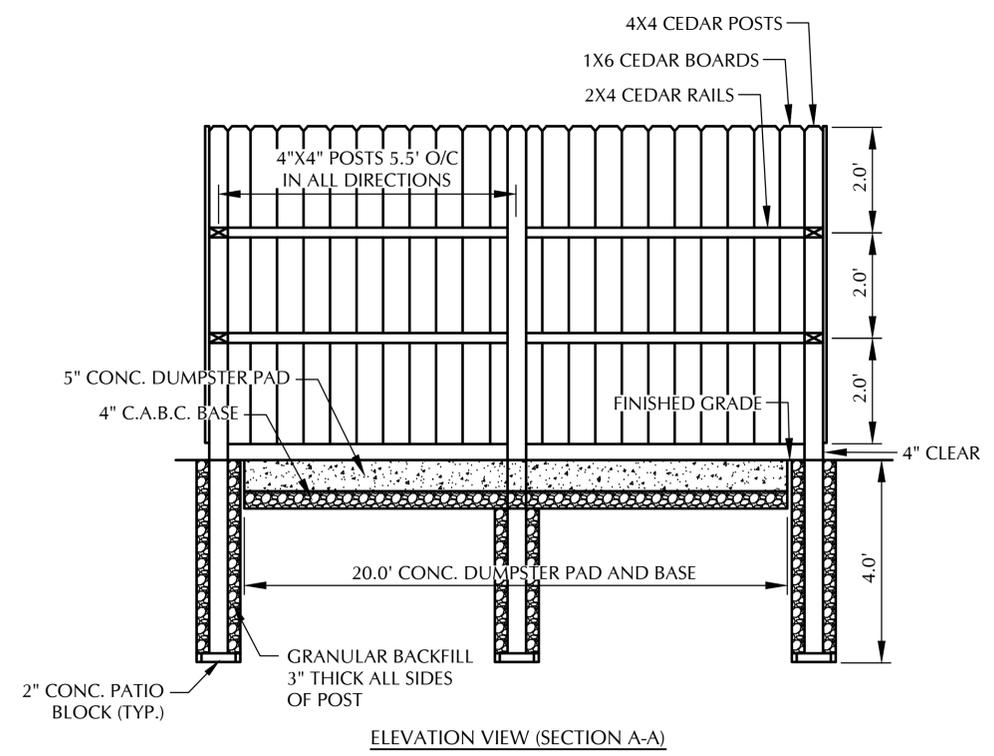
PLUG PLANTING DETAIL

SCALE: N.T.S.



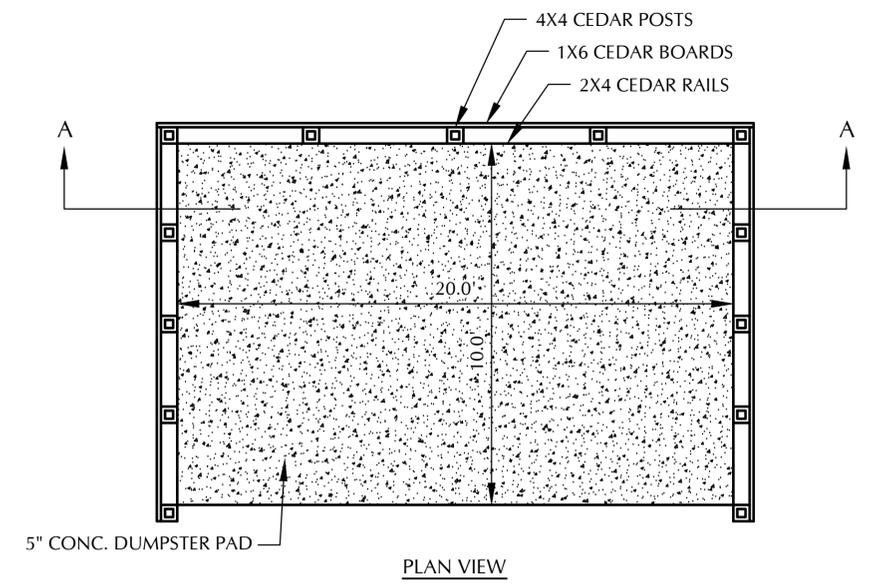
BIO-FILTRATION STRUCTURE & BASIN CROSS-SECTION

N.T.S.



ELEVATION VIEW (SECTION A-A)

- 1) CEDAR TO BE WITHOUT EXCESSIVE KNOTS, GRADE C OR BETTER CLEAR. SURFACE TO BE SAW TEXTURED.
- 2) CEDAR TO BE PRESSURE TREATED. RETENTION OF 0.30 LB/CF. WOOD TO RECEIVE THOMPSON'S BRAND "WATERSEAL ADVANCED NATURAL WOOD PROTECTOR" OR ENGINEER-APPROVED EQUIVALENT.
- 3) CONTRACTOR MAY USE NAILS OR SCREWS AS FASTENERS. ALL FASTENERS SHALL BE HOT-DIPPED OR DOUBLE-DIPPED GALVANIZED STEEL OR ALUMINUM. SCREWS SHALL BE COUNTERSUNK AND MIN. 2.5" IN LENGTH. NAILS SHALL BE 10d, 3" LENGTH.
- 4) FENCE OPENING TO FACE AWAY FROM LAKE.



TOILET/DUMPSTER PAD W/ CEDAR BOARD SCREEN FENCING DETAIL

N.T.S.

YAGGY COLBY ASSOCIATES
 ENGINEERS
 LANDSCAPE ARCHITECTS
 SURVEYORS PLANNERS
 501 MAPLE AVENUE
 DELAFIELD, WISCONSIN 53018
 262-646-6855
 FAX 262-646-6864
 EMAIL INFO@YAGGY.COM

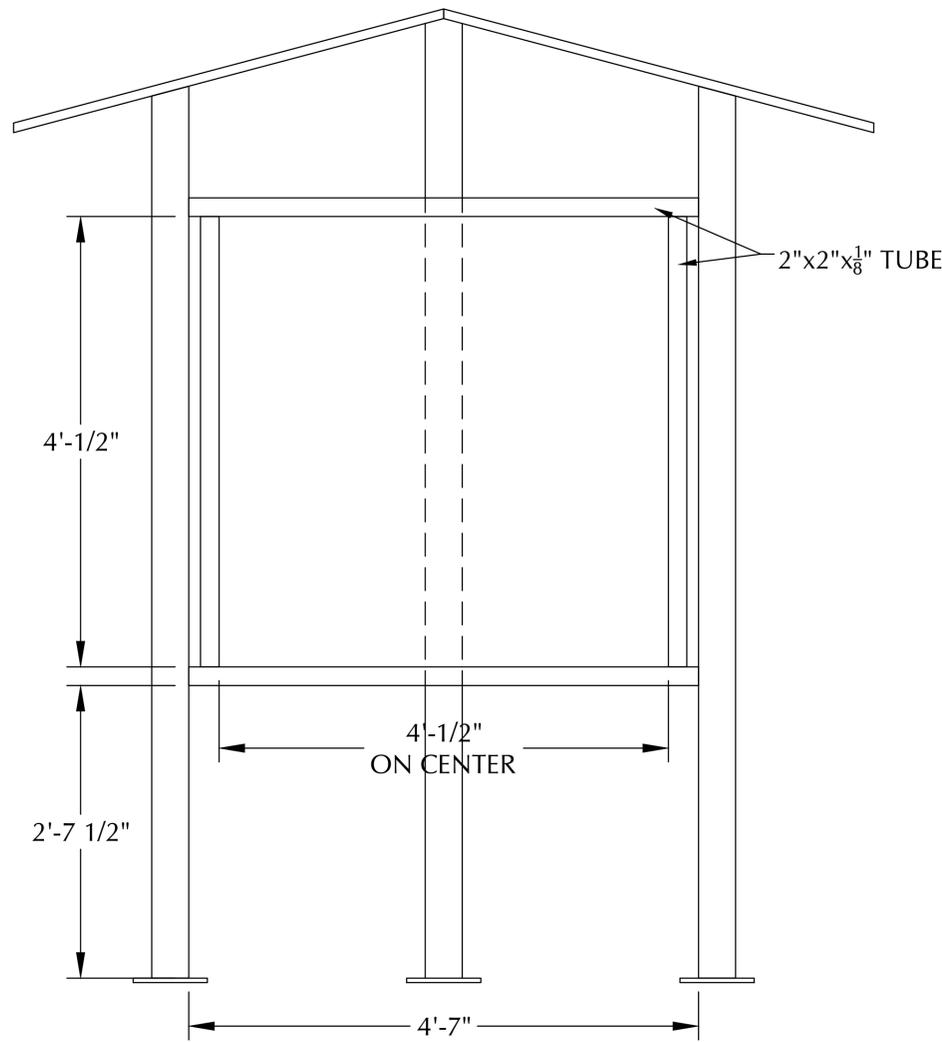
State of Wisconsin
 Department of Administration
 Division of State Facilities

SE REGION BOAT LAUNCHES - OKAUCHEE DEPT. OF NATURAL RESOURCES

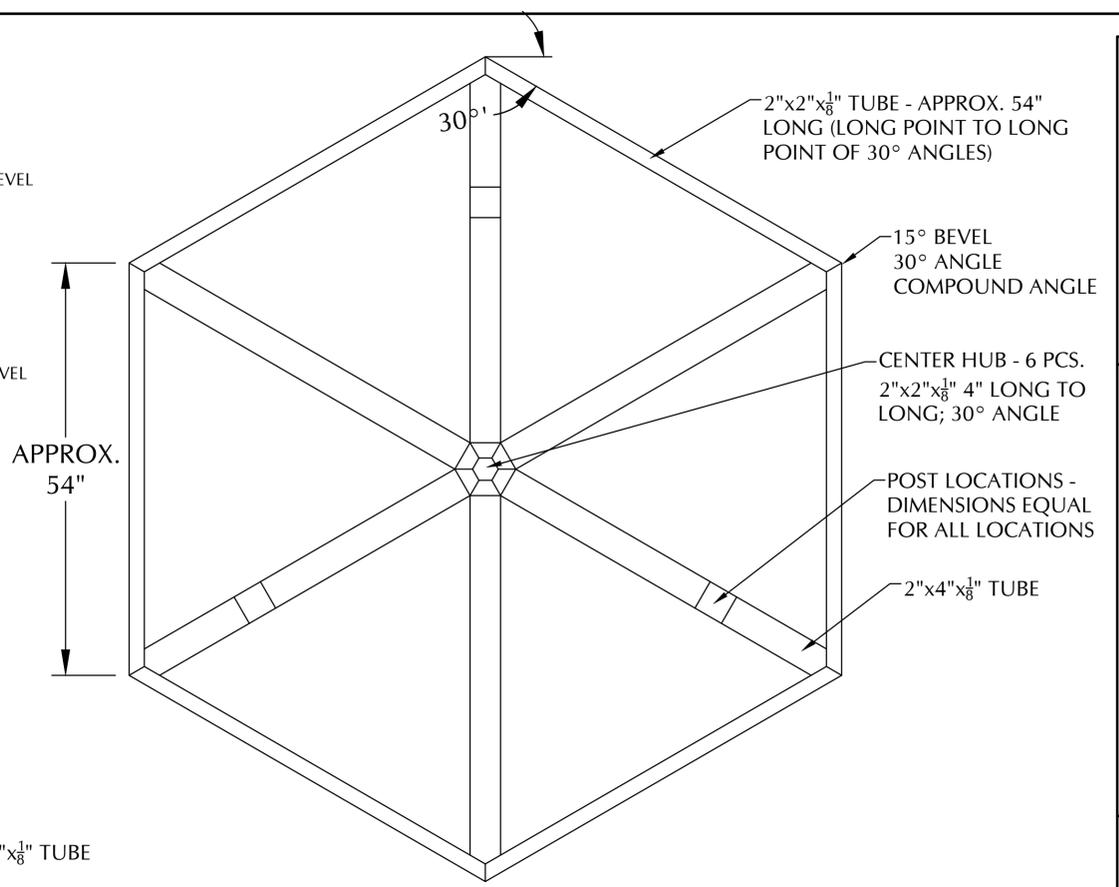
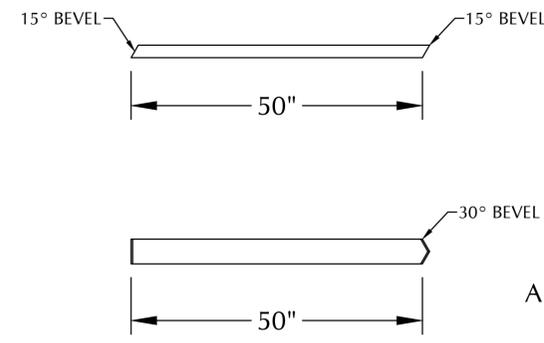
Sheet Title: GENERAL DETAILS 3

Revisions:		
No.	Date:	Description:
1	11/16/2011	DSF REVIEW COMMENTS
2	4/9/2014	DNR REVIEW

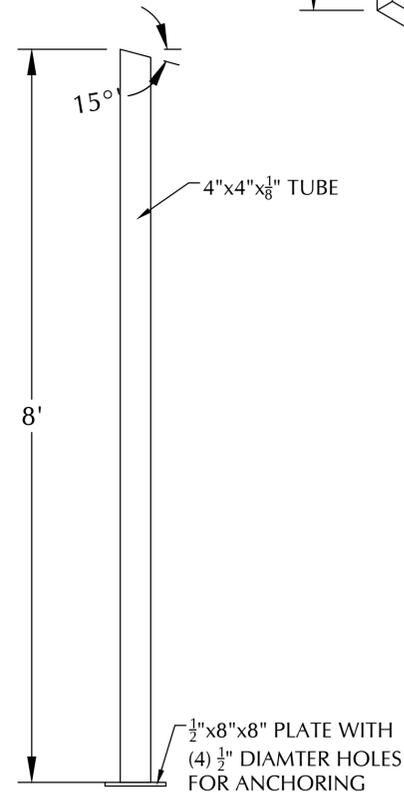
Graphic Scale	NOT TO SCALE
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CD03



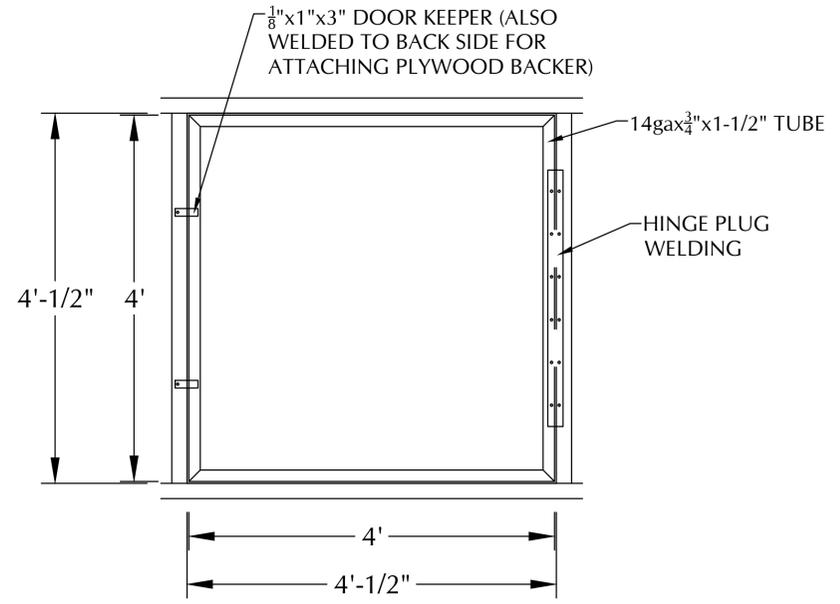
DOOR FRAME, HINGE, & LATCH



RAFTER DETAIL



VERTICAL SUPPORT LEGS



DOOR FRAME, HINGE, & LATCH

3-SIDED KIOSK MATERIALS LIST:

- 24' - 4" x 4" x 1/8" TUBE
- 48' - 3/4" x 1 1/2" TUBE
- 84' - 2" x 2" x 1/8" TUBE
- 2' - 8" x 1/2" PLATE
- 27' - 2" x 4" x 1/8" TUBE
- 1 1/2 SHEET LEXAN/PLEXI GLASS
- 1 1/2 SHEET AB OR BC PLYWOOD
- 1/2 GALLON PRIMER
- 1/2 GALLON PAINT
- 1/2 GALLON STAIN
- 25 - 2 x 6 x 8 T&G PINE
- 240 - #12 x 2 3/4" SELF TAPPING TEK SCREW
- 40 - #10 x 3/4" SELF TAPPING SCREW
- 1 1/2 - 6' x 2" HINGE
- 3 BROWN ALUM ODE 12' LONG STEEL
- 6 - 38" x 51" LONG BROWN ROOF STEEL
- 6 - VANDAL RESISTANT SCREWS
- 10' OF 24" BROWN ALUM COIL STOCK
- 1/2 POUND EACH 1" AND 2" BROWN POLEBARN SCREWS

KIOSK DETAILS

N.T.S.



YAGGY COLBY ASSOCIATES
ENGINEERS
LANDSCAPE ARCHITECTS
SURVEYORS PLANNERS
501 MAPLE AVENUE
DELAFIELD, WISCONSIN 53018
262-646-6855
FAX 262-646-6854
EMAIL INFO@YAGGY.COM

State of Wisconsin
Department of Administration
Division of State Facilities



SE REGION BOAT LAUNCHES - OKAUCHEE DEPT. OF NATURAL RESOURCES

Sheet Title:
GENERAL DETAILS 4

Revisions:		
No.	Date:	Description:
1	11/16/2011	DSF REVIEW COMMENTS
2	4/9/2014	DNR REVIEW

Graphic Scale	NOT TO SCALE
DSF Number	10H2S-01
Set Type	BD
Date Issued	01/13/2012
Sheet Number	CD04

From: [McBroom, Maureen A - DNR](#)
To: [Olson, John M - DNR \(LF\)](#); [Stock, Lance J - DNR](#)
Cc: [Radermacher, Geri M - DNR](#)
Subject: Okauchee Lake Boat Launch - NOI plan review comments & questions
Date: Wednesday, May 28, 2014 10:39:52 AM

Hello-

Here are the plan review comments on the Okauchee Lake Boat Launch plans. Please call or e-mail me if you would like to discuss these in detail.

Once these items have been resolved, I can issue the NR 216 WPDES Construction Site Storm Water Runoff Permit coverage for this site. That permit coverage is good for 3 years.

Thanks!

-McB

Some comments on the erosion control plan:

- Turbidity barrier placement on plan and timing should be identified in the sequence.
- Temporary outlet structures needed for bioretention excavations to be used as sediment traps during construction.
- The topsoil stockpile location should be shown on the plan. Stabilization within 7 days and perimeter controls around the pile should be called out on the plans.
- Show temporary diversions on north and south sides of parking lot grading to direct runoff to rain gardens (sediment traps) during construction. Add ditch checks in temporary diversions.

- Stabilization should be called out around the existing dry hydrant that will be removed.
- Will runoff get to the rain gardens, or does it need a curb cut where the 2 basins meet?
- Shoreline interface detail? What will be done as part of this project & what won't? (What is DNR work vs. contractor work?)
 - How will the shoreline / concrete work for the pier be done?

- What type of stabilization will be done along the lakefront, especially where silt fence will be installed? The disturbed areas due to the silt fence will have to be stabilized when the silt fence is removed. This item should be listed in the site-specific sequence.
- Sediment logs may be simpler to install, maintain and repair than silt fence along this area.

SWMP / SLAMM / Biofilter comments:

- Please describe how the site is meeting the 50' protective area setback.
- The narrative on page 2 of the SWMP for the bioretention facility talks about a stone storage layer. The cross section for the biofilter does not include a stone storage layer - ??
- Engineered soil void ratio in the WinSLAMM model should be no greater than 0.40. There should not be an infiltration rate assumed for the subsoil since there is high groundwater. **The SLAMM modeling for the biofilters should assume no infiltration in the subsoil due to high groundwater.** They are currently modeled with a 0.5 in/hr as it relates to the soil texture.
- **The cross section for the biofilter should be modified to provide better assurance for plant survival and better treatment for the volume that can be treated on a small storm basis. Recommend digging out the in-situ soil to a depth of 2 feet and replace bottom 6 inches with stone for a storage layer, place the underdrain in the stone storage layer to help intercept high groundwater conditions, and to increase the engineered soil planting bed depth to 18 inches instead of the 6 inches proposed. We can assign an 80% reduction for flow through an 18" filtering layer, but not 6". Increasing the planting bed depth may result in the basin bottom needing to be raised and having to pass more flow through a secondary outlet structure to keep the max water surface elevations down below the parking lot level.**
- The floodplain line should be shown on the plans.



Wisconsin Department of Natural Resources

DNR announces plans for renovating Okauchee Lake boat launch

News Release Published: September 22, 2014 by the [Southeast Region](#)

Contact(s): John Olson, DNR state boat access engineer, 608-267-7484

OCONOMOWOC, Wis. - The Department of Natural Resources is proposing to renovate the existing public boat launch on Okauchee Lake in the town of Oconomowoc.

Work at the site will include improvements to the parking lot and construction of a cul-de-sac to improve traffic flow through the site and to relieve traffic congestion on Road T. The proposed project will reconfigure parking to include a total of 23 car/trailer parking stalls and 10 parking stalls designated only for cars.

An existing vault toilet will be demolished and replaced with a concrete slab to accommodate an accessible portable toilet. Landscaping proposals include installing storm water rain gardens and a fishing pier.

No work will take place on the existing boat launch lanes or boarding dock.

This project is funded using Federal Sports Fish Restoration funds. The engineering plans are being prepared by Yaggy Colby Assoc., Delafield for the Wisconsin Department of Administration and the DNR. Construction is anticipated to begin in the spring of 2015.

The proposed department action is not anticipated to result in significant adverse environmental effects. An environmental analysis for the project has been completed and is available for review at the Oconomowoc Town Hall, W359n6812 Brown St., Oconomowoc, WI, 53066, and at the DNR Waukesha Service Center, 141 NW Barstow St., Suite 180, Waukesha, WI, 53188. Copies also can be obtained from John Olson, state boat access engineer at 608-267-7484 or Johnm.Olson@Wisconsin.gov.

Written public comments on the environmental analysis are welcome and encouraged, but must be submitted no later than 4:30 p.m. on Tuesday, October 28, 2014. Written comments can be sent to John Olson, state boat access engineer, Wisconsin Department of Natural Resources, PO Box 7921, 101 S. Webster St., Madison, WI, 53707-7921 or sent by email to Johnm.Olson@Wisconsin.gov.

The Official Internet site for the Wisconsin Department of Natural Resources

101 S. Webster Street . PO Box 7921 . Madison, Wisconsin 53707-7921 . 608.266.2621

THOMAS P. GODAR

**W343 N6475 Bayview Road
Oconomowoc, WI 53066-5134
262.966.0054**

October 27, 2014

VIA E-MAIL & HAND DELIVERY

Mr. John Olson
State Boat Access Engineer – Bureau of Facilities and Lands
Wisconsin Department of Natural Resources
Johnm.Olson@wisconsin.gov

Re: Okauchee Lake Public Boat Access Renovations

Dear Mr. Olson:

We appreciate the opportunity to have received the Environmental Analysis for Okauchee Lake Boat Access Improvements as well as the response to our September 23, 2014 letter which you provided on October 2, 2014. On behalf of the Okauchee Lake Management District (“OLMD”), I wanted to express our appreciation for your efforts. I also wanted to raise some additional questions for your consideration.

1. Access to the boat launch is frequently congested on holidays and weekends. This creates problems for neighbors as well as those who would launch boats as they drive down a single-lane roadway to the launch site only to discover others are driving back from the launch site with trailers and boats as they were unable to launch as the lot was full. Will the DNR create some sort of system of notifying would-be boaters that the launch is full and there is no access to the launch at the current time at the intersection of Road T?

2. Okauchee Lake has approximately 1,000 riparian owners. In addition, with the plans by the DNR, it will have nearly 37 car/trailer qualified sites. In addition, there are a number of non-qualified sites for boaters at the end of Lacy Lane, at the Golden Mast, and elsewhere. This creates for one of the most congested lake in southeastern Wisconsin during weekends and holidays. A number of other nearby lakes of similar size, such as Oconomowoc, Pine, and Beaver, have far fewer public qualified sites at DNR or public sites, and have far fewer riparian owners, and far less congestion. Is there a greater wisdom in expanding access to other

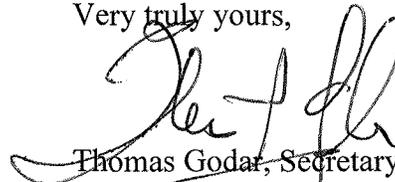
less utilized lakes at this point rather than using scarce resources to create even further opportunities for congestion on Okauchee Lake?

3. Riparian owners are often turned away at the launch site even though they are merely going to put the boat in to the lake to moor their boat at their homes. Would the DNR allow for riparian owners to launch boats so long as they are properly identified, and will not leave their car and trailer at the launch site?

4. We do not believe that the agreement is ambiguous, with regard to the number of qualified spaces allowed under paragraph 7 of the Agreement between the Wisconsin Department of Natural Resources and OLMD, and states as follows: "The total number of such spaces [qualified spaces] does not exceed 35." We believe that the plans now constitute 37 qualified spaces. The plans should therefore be reviewed and adjusted accordingly.

Thank you so much for your review of our position, and consideration of the points we raise in this communication.

Very truly yours,



Thomas Godar, Secretary
Okauchee Lake Management District

TG/vls

cc: Okauchee Lake Management District Commissioners
Sue Hildebrand
Town of Oconomowoc (Jeff Herrmann)
Waukesha County Planning

This letter intends to provide comments addressing the Environmental Analysis as published August 25, 2014, regarding the Okauchee Lake Public Boat Access Renovations to be built and improved in spring of 2015.

The respondent is: Hans Weissgerber, Jr., owner of the Golden Mast Restaurant, and private boat launching facility serving and accommodating the public with uninterrupted access to Okauchee Lake for 52 years, since 1962.

Section 1: General comment in consideration of project

Whereas the need for public access to Okauchee Lake is considerable, and highly sought for on summer weekends and holidays, the addition of 6 additional car-trailer parking stalls is reasonable and beneficial to public enjoyment of the lake.

The primary concerns for neighbors, lake residents and the community should be protected as follows:

1. The facility development must protect the aesthetic environmentally harmonious quality of a quiet, residential neighborhood.
2. Safety and protection of neighbor's private property and quiet enjoyment of life must be secured, and guarded, by providing continuous policing and supervision at all times.

Note of Correction: Golden Mast Restaurant on Okauchee Lake. This facility has the capacity to accommodate 13 (or more) car/trailer units.

Section 2: Reason for fights and altercations is:

1. Failure to continuously police and staff the launch site.
2. Failure to charge fees at all times.
3. Deliberate denial of access to Lake Residents for "in only" launching.

Recommendations:

1. Establish a payment system for all boat launching users, as currently used for Oconomowoc Lake, which is implemented and policed by the Village of Oconomowoc Lake.
2. Allow residents to launch boats anytime, subject to proof of identity as a lake resident, and no parking on site.

Note of Correction: When both private and public availability for car/trailer parking stalls is considered, more than 50 stalls are available between the DNR launch site, and Golden Mast launch site.

Section 12:

Opinion: Since the introduction of Muskellunge, a non-native fish, into Okauchee Lake, the population of bluegills, sunfish and perch has diminished dramatically. Whereas this provides a few sports fishermen the opportunity for recreational fishing, many children, families and shore fishing anglers are denied the enjoyment of casual, recreational shoreline or pier fishing.

Attachment 12: No Access Waterway

Site Status – Change ‘private’ to ‘public.’

Comment for Consideration

Whereas certain recreational lakes in the state of Wisconsin are rural, isolated, only seasonally active, are sparsely populated, and require little or no public services, other lakes, such as Okauchee Lake, are highly populated on a year-round basis and highly sought after for recreational activity by residents from either close or surrounding urban communities, therefore requiring a broader range of services to the public. This warrants consideration for a separate definition of public usage, enjoyment and benefit by defining certain lakes, or segments of lakes, as urban versus rural, or similar designation.

This type of differential may address certain usages such as commercial, multi-family residential, single-family residential, recreational or otherwise, as established by use or publicly proposed use.

Appropriate zoning ordinances could then support, promote and accommodate certain preferred uses considered in the public interest.

Examples are: Lakeshore development or public commercial such as the Convention Center in Madison, or the Conference Center in Oconomowoc, etc. Multi-family/commercial on Lake Geneva, Pewaukee, Green Lake, etc. High-density residential, recreational, commercially used lakes like Okauchee, Pewaukee and LaBelle in Waukesha County, versus low-density lakes like Pine Lake and Oconomowoc Lake.

The intent would be to recognize the difference in usage and accommodation that are characteristic, established or considered beneficial for such lakes, or lake shorelines, and the community in general

Respectfully Submitted,

Hans Weissgerber, Jr.

Owner: Golden Mast Inc.

P.O. Box 41

Okauchee, WI 53069

Email: hwjr@weissgerbergroup.com

Phone: 262-366-0029

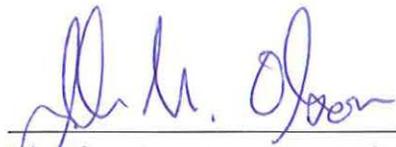
**FINDINGS OF FACT, CONCLUSIONS OF LAW, AND
DETERMINATION OF WEPA COMPLIANCE
FOR OKAUCHEE LAKE PUBLIC BOAT ACCESS RENOVATION, MONROE COUNTY**

FINDINGS OF FACT

1. In July 2008 the Department received approval from the Natural Resources Board to acquire a 0.68 acre parcel of land immediately adjacent to the south of the existing Okauchee Lake DNR boat landing located in the NE1/4, SE1/4, Sec. 35, T8N, R17E, Town of Oconomowoc, Waukesha County. This parcel of land was acquired with the intended purpose that it would be used for boat landing expansion including providing additional parking, shore fishing, and improved access to Okauchee Lake.
2. The proposed renovation required a Manual Code 3506.1 approval under Chapters 30 and 31, Stats., for alternations to navigable waters, and a permit under ch. NR 216, Wis. Admin. Code, for stormwater discharge.
3. The proposed project actions required preparation of an environmental analysis under the pre-April 2014 version of ch. NR 150, Wis. Adm. Code.
4. Under the current version of ch. NR 150, the proposed project was an equivalent analysis action and did not require preparation of an additional environmental analysis.
5. The Department prepared an environmental analysis in compliance with the requirements of the pre-April 2014 version of ch. NR 150 because the project was begun under that version.
6. The Department announced the availability of the environmental analysis for public review on September 22, 2014 and made the environmental analysis available on the Department's web site at the following address: <http://dnr.wi.gov/topic/EIA/Current.html>.
7. The public comment period closed on October 28, 2014.
8. The Department received and considered comments on the environmental analysis from two individuals.
9. In compliance with NR 150.50, the Department published the environmental analysis, public comments received and this determination on the Department's web page on October 30, 2014 at the following address: <http://dnr.wi.gov/topic/EIA/ArchiveTitle.html>.

CONCLUSIONS OF LAW AND DETERMINATION

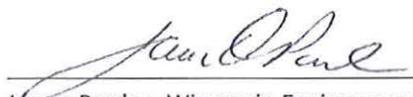
10. The Department has complied with ch. NR 150, Wis. Adm. Code, and with s. 1.11, Stats., for the Okauchee Lake boat access renovation project.



John Olson, State Boat Access Engineer
Bureau of Facilities and Lands

10-30-2014

Date



James Pardee, Wisconsin Environmental Policy Act Coordinator
Bureau of Environmental Analysis and Sustainability

10-30-2014

Date